

K. The Mill Office - Exceptional Significance

This element forms part of the Mill Building and comprises a single-storey shop that has been drastically modified with the replacement of openings, cladding and the removal of the original layout. It would be more appropriate if this were to be changed to Low Significance, given these modifications and because its original relationship to the Mill Building is no longer clearly discernible.



Figure 17: The Mill Office.

L. Manilla Street Shed - Low Significance

This is a late 20th century period shed that has less significance than other extant elements given it is a relatively recent structure. We concur with this grading.



Figure 18: Manilla Street Shed as viewed from Manilla Street.

M. Weighbridge and Siding (including industrial artefacts) - Exceptional Significance

The structure is intact; however, the significance of this element lies in the extant industrial artefacts within the shed which include rail tracks and a capstan. These items can be readily relocated or interpreted. It would be more appropriate if the shed was graded as Low Significance and the industrial artefacts contained within were graded as High Significance.



Figure 19: Weighbridge and Siding as viewed within the site.

N. Bedwells Rear Warehouse - Low Significance

We concur with this grading. It is a later element that makes little, if any, historic contribution to the site.



Figure 20: Bedwells Rear Warehouse as viewed from Manilla Street.

O. Building 16 - Low/Moderate Significance

It is agreed that this building should be graded Low/Moderate Significance. As noted by the *CMP March 2016*, the grading of Moderate Significance is specifically for the original c. 1917 building plan and layout, while the exterior is graded Low Significance.



Figure 21: Building 16 as viewed from Havannah Street.

P. Garages - Intrusive

We concur with this grading. This element detracts from the overall significance of the site.



Figure 22: Garages as viewed from within the site near Manilla Street.

3.2 Policies

Section 7.4 of the *CMP March 2016* provides general policies for the ongoing conservation of the site. These include policies for the protection of significant fabric; the interpretation of the place; adaptation of significant fabric; and any new additions to the place. The *CMP March 2016* acknowledges that the site is unlikely to ever be used as a mill again but allows for the adaptation of existing spaces for new uses; many buildings on the site are actively used for other purposes.

The proposal is consistent with these policies as it seeks to remove buildings that have less significance or are intrusive and retain those that are highly significant and clearly demonstrate their historic use. The proposal will allow its ongoing conservation and enable an improved understanding of the site. This will be achieved by adaptively reusing existing buildings and constructing new buildings for residential and commercial purposes.

The following sets out the policies provided by the *CMP March 2016* and the discussion by Weir Phillips Heritage and Planning.

1. Treatment of Fabric

Conservation guidelines for the place should determine the extent to which significant fabric should be retained and conserved. The most significant fabric should be conserved in accordance with the principles of the Burra Charter.

While all the fabric in the place can be considered significant to some extent, not all of the fabric is of such significance to warrant conservation in accordance with the principles of the Burra Charter. Levels of preservation appropriate to such fabric might include:

- *Fabric where alteration or removal is essential to maintain the continued historical use of the site for the purposes of healthcare or community uses;*
- *Fabric where alteration or removal is necessary for the conservation or interpretation of the place;*
- *Reconstructed fabric where alteration or removal is desirable to create more accurate reconstruction of elements of the place;*
- *Fabric where alteration or removal is optional; and*
- *Fabric where alteration or removal is desirable.*

While any significant fabric remains, it should be maintained. Any fabric reconstructed in accordance with the conservation policies should also be maintained. Views to the place and from the place that are defined by fabric or contribute to the significance of the place should be maintained and either protected from change or re-established.

Weir Phillips Heritage and Planning response:

The client proposes to adaptively re-use significant spaces that have been designated Exceptional or High and to remove others that have lesser significance. This document has examined the existing buildings in detail and made recommendations to change the grading of some elements. The retention of highly significant buildings and the removal of elements with less significance or that are intrusive will allow for an improved understanding of the historic use of the site and better enable the proposed new uses.

2. Interpretation of the Place

An interpretation strategy should be developed to commemorate the history and evolution of the site including changes to the curtilage and usage. Commission the services of a suitably qualified company specialising in interpretation strategies to develop a scheme and incorporate all signage and plaques at the place to display historical data. The Interpretation Strategy should include the following:

- *Appropriate mechanisms for interpretation;*
- *Principal themes for interpretation;*
- *Aspects of the place of outstanding significance which should be emphasised; o Configurations which are most appropriate for the fabric;*
- *Specific conservation opportunities;*
- *Restoration/reconstruction works essential to the understanding of the place; and*
- *Levels of public and specialist access.*

Weir Phillips Heritage and Planning response:

An Interpretation Strategy and Plan should be implemented in accordance with Heritage Office Guidelines to allow for an understanding of the original use and history of the site and any buildings on the site prior to its redevelopment.

3. Use of the Place

The significance of the former Tremain's Flour Mill site is embodied in its historical use as a flour mill and associated uses of a store selling the mill s products. The site and the various buildings have been used in the storage of grains and in milling the grains to produce fine and coarse flour and other by products. The site has also been used as a residence, offices, and stores along with a small scale flour mill. The layout of the mill complex has remained intact after the sale of the site. The flour milling operations ceased in 1980. Although the mill and the sheds are rented out to various businesses, only some equipment is in use. It is highly unlikely that the site will be reused for its original purposes as a flour mill. Tremain's Flour Mill is highly significant as the longest running mill in Bathurst. The various buildings, structures and equipment are of high significance due to their association with the Tremain family and for the flour milling operations. There are possibilities where these structures can be reappropriated for a different and appropriate use, provided the spaces are made suitable for the new use. Finally, in all future designs for the buildings where modifications are required, all attempts should be carried out without any compromise to the identified cultural significance (in this CMP) of the various structures, to integrate new uses and services as required for the ongoing usage of the entire site.

Weir Phillips Heritage and Planning response:

The buildings on the site can be successfully adapted for new uses in recognition of the fact that it is unlikely that the site will ever be used as a mill again. Fabric that has been identified by this document to have less significance can be safely removed to facilitate modification as required.

4. Intervention in Significant Fabric

Conservation guidelines should identify areas where significant fabric can be disturbed. The degree of professional involvement appropriate to any disturbance should also be identified. Appropriate levels of investigation of the fabric for conservation of the place and for scientific research should also be addressed. Refer to section 5.4 and Figure 275, which provides a list of all physical elements and grades of significance.

Weir Phillips Heritage and Planning response:

The proposal will follow the necessary conservation guidelines to protect significant fabric.

5. Adaptation of Significant Fabric

The most extensive intervention into significant fabric is likely to occur if new uses are being accommodated at the place. Depending on the level of significance of the fabric, different levels of alteration are appropriate and may include:

- *Conservation in accordance with the Burra Charter;*
- *Partitioning of internal spaces; and*
- *Removal of intrusive elements identified.*

Weir Phillips Heritage and Planning response:

This Addendum has identified intrusive elements that could be safely removed to facilitate the proposal without detracting from the overall significance of the site. Given the various ratings of significance of buildings on the site, different levels of alteration are appropriate and may include:

- Conservation in accordance with the Burra Charter;
- Rearrangement of internal spaces; and
- Removal of intrusive elements identified.

6. Additions and New Buildings and Other Features at the Place

Guidelines should address the design of new buildings and additions. Additions should be defined in location, form, height, bulk and the effect they have on existing fabric. New buildings and features might be detrimental to the place and its setting and should also be defined in terms of their location, form, height, bulk and their effect on views to and from the place. New elements at the place such as free standing structures, temporary structures, outdoor furniture, roads and car parks, and floodlighting may need to be addressed. The Bathurst Regional Council's LEP and DCP will provide guidelines on additions and new buildings near heritage places.

Weir Phillips Heritage and Planning response:

The proposal is accompanied by a Concept DA that is respectful and appropriate within its architectural and historical context.

Guidance to address the design of new buildings and additions should be defined in terms of location, form, height, bulk and the effect they have on existing fabric. New buildings should also be defined in terms of their location, form, height, bulk and their effect on views to and from the place. New elements such as free-

standing structures, temporary structures, outdoor furniture, roads and carparks, and floodlighting will need to be addressed. The Bathurst Regional Council's LEP and DCP will provide guidelines on additions and new buildings on and near heritage places.

7. Conservation Procedures at the Place

As the place is of high cultural significance, procedures for managing change and activities should be managed by recognised conservation methodologies such as the Australia ICOMOS Burra Charter.

Weir Phillips Heritage and Planning response:

This statement is acknowledged.

8. Adoption and Review of Conservation Guidelines

The conservation guidelines should contain recommendations about the adoption of the guidelines. Because the place will change over time, the conservation guidelines should be reviewed periodically to ensure they continue to address the significance of the place and the constraints and opportunities of the place.

Weir Phillips Heritage and Planning response:

This document has made several suggestions to amend the Grading of Significance which recognises the significance of the place and allows for adaptive reuse of existing buildings and the construction of complementary new buildings as appropriate.

9. Review and Adoption of this CMP

The recommended policies and actions specified in this CMP are to be reviewed by Bathurst Regional Council and after all revisions should be adopted as the guide for all future work at the place.

Weir Phillips Heritage and Planning response:

This Addendum provides several amendments which should be adopted as the guide for future work at the place.

10. Impact Assessment required for all Proposed Works

Prior to undertaking work on any of the fabric identified in this plan, a Statement of Heritage Impact must be prepared in accordance with the Heritage Office's current guidelines. Any new work proposed for a building, its curtilage, or the complex in general, will need to demonstrate that it is being carried out in a manner that is respectful of the heritage significance established for the complex/ building and its setting in Section 5.3 – Statement of Significance.

Weir Phillips Heritage and Planning response:

The proposal will be accompanied by a Heritage Impact Statement prepared by Weir Phillips Heritage and Planning that assesses the impact of the proposed works, who have been engaged as the professional consultants to advise on the proposal through all its stages.

11. Involvement of Heritage and Conservation Professionals

Conservation works undertaken in accordance with this CMP should involve experienced heritage and conservation professionals. This CMP should be reviewed at five yearly intervals or alternatively at such a time that major changes are proposed for the former Tremain's Flour Mill site.

Weir Phillips Heritage and Planning response:

The CMP March 2016 was published over six years ago. This Addendum, dated February 2023, has reviewed it, and suggested several amendments to the Grading of Significance that will better facilitate the proposal while enabling an improved understanding of the place.

12. Archaeological Assessment

Prior to any proposed future works involving excavation, an archaeological assessment is to be carried out to determine the potential discovery of artefacts and/ or relics. In the event that such discovery is plausible, a Section 167 or 140 (Heritage Act 1977) Excavation Permit should be obtained from the Heritage Office prior to commencing any work. An archaeological investigation would then need to be carried out.

Weir Phillips Heritage and Planning response:

An archaeological assessment should be carried out.

13. Proposed Alterations to Fabric of High Significance

Any changes proposed to the fabric and elements identified in this report as being of exceptional or high significance must be minimal and must not result in a reduction of their significance. Such spaces and elements must not be demolished and appropriate maintenance of them must be undertaken. Any new work proposed to these highly significant spaces and elements must be sympathetic to the original fabric and any modifications to such fabric is to be subject to a formal Statement of Heritage Impact in accordance with the Heritage Office guidelines. Refer to Section 5.4 and Figure 275 which provides a list of all physical elements and grades of significance.

Weir Phillips Heritage and Planning response:

All fabric of High or Exceptional Significance undergoing change will have that change kept to the minimum required and will not result in loss of overall significance. New works for adaptive re-use will be sympathetic and any modifications to such fabric is to be subject to a formal Statement of Heritage Impact in accordance with the Heritage Office guidelines.

14. Proposed Alterations to Fabric of Moderate Significance

Proposed changes to fabric identified in this report as being of moderate significance may take place so long as it does not result in a reduction of the significance constituted in the spaces and elements identified in this report as possessing high significance.

Demolition of such spaces or elements is generally permissible where appropriate. Any new work proposed to such spaces identified as possessing moderate significance should, wherever possible, be sympathetic to the original fabric and spaces. Any modification to fabric of moderate significance is to be subject to a formal Statement of Heritage Impact in accordance with the Heritage Office guidelines. Refer to Section

5.4 and Figure 275, which provides a list of all physical elements and grades of significance.

Weir Phillips Heritage and Planning response:

All fabric of Moderate Significance undergoing change will have that change kept to the minimum required and will not result in loss of overall significance. Demolition of such spaces or elements is generally permissible where appropriate. New works will be sympathetic and any modifications to such fabric is to be subject to a formal Statement of Heritage Impact in accordance with the Heritage Office guidelines.

15. Proposed Alterations or Removal of Fabric of Low Significance

Proposed changes to fabric identified in this report as being of low significance may take place so long as it does not result in a reduction of the significance constituted in the elements and spaces identified in this report as possessing high significance.

Demolition of such spaces or elements is generally permissible where appropriate. Any new work proposed to such spaces that is identified as possessing low significance should, wherever possible, be sympathetic to the original fabric and spaces. Any modification to fabric of low significance is to be subject to a formal Statement of Heritage Impact in accordance with the Heritage Office guidelines. Refer to Section 5.4 and Figure 275, which provides a list of all physical elements and grades of significance.

Weir Phillips Heritage and Planning response:

All fabric of Low Significance undergoing change will have that change kept to the minimum required and will not result in loss of overall significance. Demolition of such spaces or elements is generally permissible where appropriate. New works will be sympathetic and any modifications to such fabric is to be subject to a formal Statement of Heritage Impact in accordance with the Heritage Office guidelines.

16. Proposed Removal of Intrusive Fabric

Proposed changes to fabric identified in this report as being ‘intrusive’ may take place so long as it does not result in a reduction of the significance constituted in the elements and spaces identified in this report as possessing ‘exceptional’ or ‘high significance’. Such intrusive spaces and elements should be removed or modified to eliminate or minimise their detrimental impact on the significance of the former Tremain’s Flour Mill site. Any removal of intrusive fabric is to be subject to a formal Statement of Heritage Impact in accordance with the Heritage Office guidelines. Refer to Section 5.4 and Figure 275, which provides a list of all physical elements and grades of significance.

Weir Phillips Heritage and Planning response:

All fabric identified as Intrusive may be removed provided it will not result in loss of overall significance. Demolition of such spaces or elements is permissible and where it detracts from fabric or spaces that are of significance, removal is appropriate and encouraged. New works will be sympathetic and any modifications to such fabric is to be subject to a formal Statement of Heritage Impact in accordance with the Heritage Office guidelines.

17. Statement of Heritage Impact

Any alterations proposed in relation to preparation of the place for domestic habitation is subject to undertaking a formal Statement of Heritage Impact in accordance with the Heritage Office guidelines. Any future modifications are to be carried out strictly in accordance with the conservation policies contained in this Plan. All works are to be presided over by the appointed heritage architect whose role it will be to regularly report back to the relevant authority (in advance of any works taking place within or on the building) of intended modifications to fabric identified in this CMP as being of moderate or high significance.

Weir Phillips Heritage and Planning response:

Any such alterations will be accompanied by a formal Statement of Heritage Impact in accordance with Heritage Office guidelines. Future works will be carried out in accordance with the conservation policies in conjunction with an appointed heritage architect where modifications are required for fabric of moderate or high significance.

18. Tradespeople

All future works undertaken at former Tremain's Mill site should be carried out by suitably qualified and experienced tradespeople. Reference should be made to the Heritage Office list of qualified tradespeople for each trade – refer to the Heritage Office website and the Bathurst Regional Council's website as well.

Weir Phillips Heritage and Planning response:

All future works that are undertaken at the site are to be carried out by suitably qualified tradespeople who have a high level of knowledge and experience in working with heritage buildings.

4 CONCLUSION

This Addendum to the *CMP March 2016* examines the current Grading of Significance and Policies for the site. The *CMP March 2016* is often inconsistent in the way it grades elements; it tends to be contradictory to the established history of the structures, and the criteria used for the Grading of Significance. This can be seen in the grading given for the Mill Building, which has undergone considerable alteration. The new gradings properly recognise the relative importance of each individual element and allow for a better understanding of the use of the site and enhance its heritage values.

Slight modifications to the Policies are designed to ensure a high quality and amenity of adaptive reuse while ensuring heritage significance is maintained and where possible, enhanced.

This Addendum to the *CMP March 2016* is carried out in anticipation of a thorough restoration and appropriate adaptive reuse of the Former Tremain's Flour Mill site.

HERITAGE IMPACT STATEMENT



Concept Development Application

Former Tremain's Flour Mill, No. 7 Keppel Street and No.
67 Havannah Street, Bathurst

February 2023 | J5675

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Cover Image: Former Tremain's Flour Mill from inside the Keppel Street entrance.

We acknowledge that the land on which we live, learn and work as the traditional country of the Gadigal people of the Eora Nation. We acknowledge these traditional owners of this land and acknowledge their living cultures and the unique roles they have played in maintaining life, language, and culture in this region. We pay respect to their Elders past, present and emerging and all Aboriginal people.

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1 INTRODUCTION

1.1 Preamble

This Heritage Impact Statement (HIS) has been prepared in conjunction with a Concept Development Application for the redevelopment of the Former Tremain's Flour Mill located at No. 7 Keppel Street and No. 67 Havannah Street, Bathurst, New South Wales.

True Green Group has prepared a Concept Development Application providing a master plan to guide future development of the former Tremain's Flour Mill located at 7 Keppel Street and 67 Havannah Street, Bathurst.

The masterplan provides potential land uses, spatial distribution of the buildings, heights, setbacks, massing and landscape concept. The Concept Development Application only seeks consent for maximum building envelopes and all built form will be subject to future detailed Development Applications that will be designed within the building envelopes.

The proposed development will transform the former Tremains Flour Mill and surrounding precinct into a thriving mixed use adaptive reuse project. This will include commercial and educational spaces, food, beverage and retail with publicly accessible spaces and some residential dwellings.

The adaptive re-use of the precinct includes upgrades, alterations and additions to existing buildings of heritage significance – The Mill Building, Bedwells Barn, Timber Silos and Concrete Silos. There will also be various new complementary buildings within the existing Tremains Flour Mill precinct.

The Concept Development Application and proposed Master Plan provides a range of opportunities for site development including:

Hotel Accommodation

Various accommodation across the site located in a combination of new and adaptive re-use buildings comprising:

- 9 room heritage boutique hotel in existing 1880s Victoria Stores Building. Development Application approved.
- 51 room new boutique hotel fronting Keppel St with rooms ranging from 32sqm to 46 sqm.
- 52 room new boutique hotel within the Havannah St precinct with rooms ranging from 33 sqm to 45 sqm.
- 10 room boutique hotel in existing concrete silos with 20sqm sized rooms.

Commercial

Range of commercial offerings across the site comprising:

- Hotel reception, guest library and lounge.
- Arts exhibition space.
- Farmers Market.
- Bedwells Barn catering for a regular farmers market and multi-functional wedding and event space.
- Commercial kitchen.
- 240 sqm roof top bar located at the roof level at the existing concrete silos.

- 90 seat art-house cinema and event space.
- Office, gallery, exhibition, studio space over four floors of commercial space plus associated 6 x 1 bedroom artist in residence studios.

Retail

Wide range of complementary mixed-use retail, food and beverage offerings comprising 28 tenancies:

- Cafes
- Enoteca
- Restaurants
- Distillery
- Assorted bespoke retail – bookshop, small goods, beauty salon, haberdashery, hardware, homewares, flower shop, hair salon, wellness centre.
- Assorted bespoke food and beverage – artisan bakery, fromagerie and delicatessen, butchery.

Residential

Various residential offerings at the rear of the site comprising:

- 12 x 3 bedroom plus study terraces facing Manilla Street
- 4 x 2 bedroom plus study mews

Carparking

- 46 x on site car spaces
- 21 x angled car spaces on Havannah St frontage.

The site is located within the Bathurst Regional Council area. The principal planning control for the site is the *Bathurst Regional Local Environmental Plan 2014 (LEP 2014)*. The site is listed as a heritage item ('Tremain's Mill & Silos (formerly Victoria Mill) & Invincible Flour Sign') and is located within the Bathurst Heritage Conservation Area as defined by Schedule 5 Parts 1 and 2 of the *LEP 2014*. The site is located within the vicinity of heritage items listed by this Schedule and items listed on the State Heritage Register under the *NSW Heritage Act 1977*.

Under Part 5.10 of the *LEP 2014*:

(4) Effect of proposed development on heritage significance

The consent authority must, before granting consent under this clause in respect of a heritage item or heritage conservation area, consider the effect of the proposed development on the heritage significance of the item or area concerned. This subclause applies regardless of whether a heritage management document is prepared under subclause (5) or a heritage conservation management plan is submitted under subclause (6).

(5) Heritage assessment

The consent authority may, before granting consent to any development:

- (a) on land on which a heritage item is located, or
- (b) on land that is within a heritage conservation area, or

(c) on land that is within the vicinity of land referred to in paragraph (a) or (b), require a heritage management document to be prepared that assesses the extent to which the carrying out of the proposed development would affect the heritage significance of the heritage item or heritage conservation area concerned.

This statement has been prepared at the request of the owner of the site and accompanies plans prepared by Karl Walls Design.

1.2 Authorship and Acknowledgements

This HIS was prepared by Elliot Nolan, B.A. (Anc.Hist.Hons), M. Mus.Herit.Stud., Grad.Dip.Herit.Cons., and James Phillips, B.Sc. (Arch.), B.Arch., M.Herit.Cons. (Hons), of Weir Phillips Heritage and Planning.

1.3 Limitations

The history contained in this statement was prepared using the readily available resources listed under Section 1.6 below.

No Aboriginal or historical archaeology was carried out on the site.

1.4 Methodology

This assessment has been prepared with reference to the *NSW Heritage Manual* update *Statements of Heritage Impact* (2002) and with reference to the Council planning controls listed under Section 1.6.

1.5 Physical Evidence

A site visit was carried out in October 2022. Unless otherwise stated, the photographs contained in this statement were taken at this time.

1.6 Documentary Evidence

1.6.1 General References

- *Conservation Management Plan – Tremain's Flour Mills, Bathurst NSW* (2016) by Senthilpavai Kasiannan.

1.6.2 Heritage Listing Sheets

- 'Bathurst Railway Station, Station Masters Residence, Cottage, Railway Institute and Warehouse Buildings', Heritage NSW State Heritage Inventory ID No. 1080695.
- 'Centennial Terrace', Heritage NSW State Heritage Inventory ID No. 1080156.
- 'Havannah Street Group', Heritage NSW State Heritage Inventory ID No. 1080112.
- 'Loxley House', Heritage NSW State Heritage Inventory ID No. 1080155.
- 'Terraces', Heritage NSW State Heritage Inventory ID No. 1080158.
- 'Victoria Hotel', Heritage NSW State Heritage Inventory ID No. 1080164.
- 'Webb's Commercial Building', Heritage NSW State Heritage Inventory ID No. 1080157.

1.6.3 Planning Documents

- *Bathurst Development Control Plan 2014*.
- *Bathurst Local Environmental Plan 2014*.

1.7 Site Location

No. 7 Keppel Street and No. 67 Havannah Street is located on the east side of Keppel Street and the north side of Havannah Street and has frontage to Manilla Street (Figure 1). The

site is identified Lot 11, D.P. 1266801 (No. 7 Keppel Street) and Lot 12, D.P. 1266801 (No. 67 Havannah Street).



Figure 1: Map of former mill precinct. The subject site is outlined in red.
SIX Maps, 2023

2 BRIEF OUTLINE OF THE HISTORICAL DEVELOPMENT

The following history of the site and area is obtained from the *CMP 2016*.

2.1 Aboriginal History

Prior to European settlement, Wiradjuri people occupied the Bathurst region. They occupied the Macquarie River area on a seasonal basis. Initially, European intrusion into Wiradjuri country was restricted on the orders of Governor Macquarie. From the early 1820s these restrictions were removed and the focus of the settler attention was on areas favoured by the indigenous people; causing the dislocation of the Wiradjuri with the arrival of pastoral settlers and their herds in greater numbers, culminating in open conflict in 1823 and 1824.

By 1840s there was widespread dislocation of the Aboriginal people. Further destruction to the indigenous population was caused due to disease, alcohol and the gold rush. From the 1890s, the remaining indigenous population were placed on reserves and missions outside the Bathurst area.

2.2 Brief History of Bathurst

Following the crossing of the Blue Mountains, the Central Tablelands were found to be good as potential grazing land for the herds of the settlers. In the beginning, private occupation was restricted to the east bank of the Macquarie and Campbell rivers. Bathurst was a government station from 1815. In the 1820s and 1830s, more and more convicts completed their sentences and sought land. Combined with free migrants qualifying for larger land grants, the number of settlers moving west increased. As

more Europeans moved to the west, the need for villages and market towns grew and Bathurst was laid out in 1833 and before the 1850s other towns including Mudgee, Rylstone, Orange, Wellington and Carcoar were laid out.

The discovery of gold in the 1850s had a very dramatic impact on the Central Tableland. The gold rush brought a massive influx of people into the area from 1851 onwards. The arrival of the railways in the late 19th century further reinforced the growth of Bathurst as an industrial and service centre.

2.3 Wheat Cultivation in Bathurst

Wheat is one of Australia's important agricultural commodities and it forms a staple food for more than half the world's population. Wheat industry had its beginnings over 200 years ago when Governor Phillip, instigated farming operations. However, as the soil at Farm Cove was sandy and poor, in 1788, he established a 400 acre farm in Parramatta and enabled ex-convicts to own their own farms. James Ruse was the first and best-known ex-convict farmer with his Experiment Farm in Parramatta. By the end of 1790, 200 bushels (approximately 5.4 tonnes) of wheat had been harvested, all of which was saved for seed. With the opening up of Liberty Plains (now two of Sydney's western suburbs, Homebush and Strathfield) by free settlers, the colony had 6,000 acres under wheat by 1799. Although there were many hurdles initially, advancements in agricultural practices and research into newer varieties of wheat helped advance the growth of the golden grain.

Bathurst and its surrounding areas saw much of the beginnings of the Australian colonial settlement. Bathurst being, Australia's first inland settlement, it is also Australia's oldest inland city. Once the explorers had conquered the mountains, Australia's future was secured. So began the natural route to the west. Bathurst Plains had the best grass of all "excellent good land", said George Evans. Now properties across the plains support cattle and sheep, and grow wheat, vegetables and fruit. Governor Lachlan Macquarie recorded the following in his journal after the 1821 tour of Bathurst:

He [Lt. William Lawson] has 150 acres of as fine wheat now growing as I ever beheld in any country, 15 acres of very fine looking oats, and 3 acres of very good flax now in the ground, and almost ripe for cutting belonging to Government. The Macquarie wheat thrives well here, and upwards of 50 acres of it has been sown this year.

Prior to the 1870s and 80s, most of the wheat that was grown in NSW was farmed on the coastal ranges. After this time there was a marked change to inland farms and by 1881, ninety-five per cent of the wheat acreage was on inland farms. In the 1890s, there was a boom in wheat production and by 1898; New South Wales became a consistent wheat exporter rather than importer. This transformation in the wheat industry in the last decade of the nineteenth century has been attributed to: a rising population of farmers; a technological revolution in cultivation, which achieved general acceptance in this decade; the extension of the railway network into the wheat lands; the adoption of suitable wheat varieties for hot, dry conditions; and the evolution of land tenures that were suitable for wheat farmers. The wheat belt ran from the Spencer Gulf in South Australia, through the Wimmera and Mallee of Victoria, into central New South Wales. The concentration of wheat farms in NSW in the late nineteenth century was in the south-central area of the State. By the turn of the century, the concentration was moving north towards the Darling Plains. By the mid twentieth century, Moree had become the centre of a vast wheat growing region in north central NSW. Around fifty flour mills are listed in the Greater Western region and possibly a greater number existed in the early to late twentieth century.

2.4 Flour Milling

As the production of wheat increased, the need to mill the flour also increased. Prior to modern technology of grinding, for hundreds of years pairs of stones were used for grinding wheat. However, this system produced a fine mixture of finely powdered bran, germ and endosperm, which could not be effectively separated using equipment. Moreover, the necessity for white flour for purposes of baking fine bread drove the development and refinement of

flour milling. White flour was considered superior and a sign of luxury in the 18th century England.

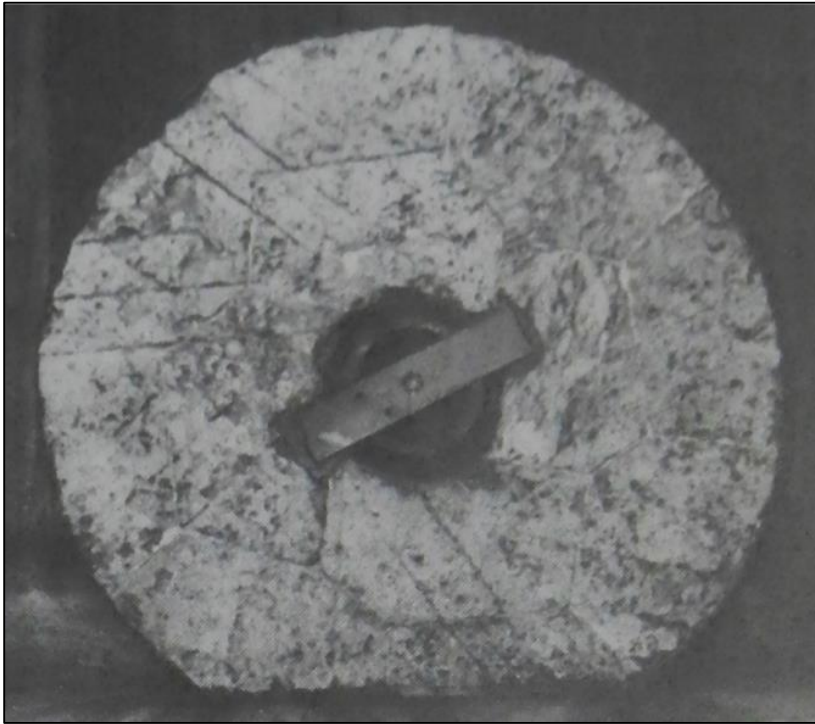


Figure 2: French burr millstone used on the Rainham property is in the yard of the Tremain's Mill.

100 Years of Milling, p. 11.

The French Burr Stone for grinding wheat into flour incorporates a buhrstone, which was a variety of quartz containing many small empty cells, giving it a rough surface.

The circular millstones were made out of twelve or more cut stones inlaid together to form a perfect circle held together by one or two heavy iron rims. The centre would be either circular or square depending on the type of shaft.

As milling technology developed and improvements were made to milling and sifting processes, white flour became easily available at a reasonable cost to all. Further, the advent of steam power in the early nineteenth century enabled larger mills and the continuous production of flour.

In the 1850s, stone mills were developed in the wheat growing areas west of Sydney. Many flourmills were built throughout the wheat belt and the total number of mills in Australia in 1908 numbered 235. In the period 1900–1930, the harvests increased and Australia became a major flour exporter. In 1916 the Grain Elevator Act was passed and this provided for erection of wheat terminals for storage of the grain. In 1939 Australian Wheat Board was established to market the wheat crop in Australia (for flour mills) and overseas. In 1951, flour exports peaked and by 1958 flourmills started to diversify into bread baking, stockfeed milling, semolina and starch production. However, by 1960–1980 the flour exports started declining and there was a decline in the number of operating mills.

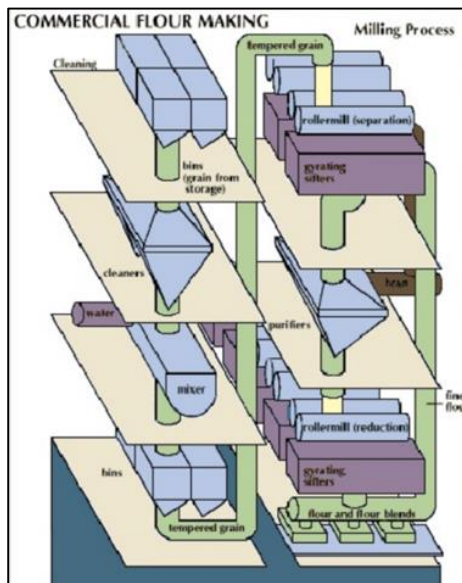


Figure 6: Flour Milling Process.
Presentation by G. Williams, 2012

2.5 Remains of Bathurst

The Tremain family in Bathurst originally came from Cornwall, and they belong to a long lineage of Tremains. According to the family history compiled in 1989, there are 'ancient deeds' held by the Public Office, London with the name Tremayne dating to as far back as the fourteenth and fifteenth centuries. The Tremain legacy is therefore probably an ancient one.

William Tremain, born in 1831 in England had served as a flour miller from a very young age. He had worked in Knight's Mill at Upper St. Teath Parish, where stone grinding system was used to grind flour. He lived with his wife (Elizabeth Westlake, his second wife) and they lived in a stone house a short distance from the mill.

At the age of 25, W. Tremain set sail for Australia along with his wife, son and daughter aboard the Sardinian in 1856. Owing to his experience as a flour miller, he sought to work in flour mills. On arriving in Sydney, William travelled to Dungog for his first job. He had to return to Sydney due to the bad conditions at the mill and decided to move to another job in Mitchell's Creek in Bathurst. The railway line had been completed to Penrith, and the family had to make their way by road that was created by William Cox in 1815. William's early job was to work in a quartz crushing machine and the family stayed in a slab hut roofed with canvas.

However, William Tremain, keen on continuing the flour milling, partnered with his nephew Bonnear and using his and his wife's savings, started a flour milling business in 1863. Bonnear's two-stone mill was in South Bathurst. This part of Bathurst was also known as Mill town in those days. It stood at the corner of Lambert Street and was called Crealy's Mill. Although they had a 50 year lease, due to bad luck and crops failing, the venture ran into trouble. Despite his troubles, William was able to install some basic flour milling machinery in Rainham and was soon back in the business. The work was tough, but they pulled through with the help of Mr Mugridge, his benefactor. However, trouble came through the expansion of the railways. The mill property lost road access when a deep cutting was put in front of the mill, forcing Tremain's family to move. Eventually, good fortune favoured Tremain in the form of Mr F.W. Smith, who owned a flourmill in Keppel Street in Bathurst along with his brother. Victoria Mill in Keppel Street was sold to Tremain in 1874 for £1600.



Figure 7: William Tremain.
Tremain Family Archives



Figure 8: Tremain Family in 1886.
Tremain Family Archives

2.6 Rate Records and Ownership – Victoria Mill / Tremain’s Mill

<p>1850-1854</p>	<p>1850: John Burge – Lot 9 (Grant by purchase)</p> <p>1852: Samuel Down – Lot 11 (Grant by purchase) – 30 pounds</p> <p>1854: Francis McMahon – Lot 10 (Grant by purchase) – 53 pounds.</p>
<p>1857-65</p>	<p>From 1857 to 1865, the Chapman brothers owned the place. The rate records list a Flour Mill in ½ acre.</p> <p>The land was leased to J.S. Brown who ran a ‘Brown’s Mill’ on the site.</p> <p>An ad in the newspaper in 1860 states the name of the mill as Victoria Steam Mills from 1860. The site also recorded a brick house during this period.</p>

1865-74	<p>F.W. Smith and Samuel F Smith (Brothers)</p> <p>Smith brothers owned the property and they ran the Victoria Mills. A brick house was also recorded on the site. An advertisement for the sale of the mill stated that: "The Victoria Mill (lot 11, Section 86) along with a Brick cottage of four rooms, brick two-room kitchen and other outhouses. The mill run on steam also had a boiler. The railway station was proposed to be opposite to the mill."</p>
1874-98	<p>On 13th of January 1874, William Tremain purchased Victoria Mill for 1600 pounds. The estimated annual income for the property was 180 pounds. He also rented a 3-roomed lath and plaster house with land adjacent to the Mill from Francis McMahon.</p> <p>In the late 1880s Simon's roller mills were installed in both Victoria and Britannia Mills.</p> <p>The founder of the mill William Tremain had served on the Bathurst City Council in 1895-97.</p> <p>William Tremain died in 1898.</p>
1899	<p>Following the death of W. Tremain, the management of the mill and other businesses came under the three sons.</p>
1900-25	<p>Fire gutted the mill in 1901 and milling operations shifted to Britannia Mill while Victoria Mill was rebuilt.</p> <p>By 1902 Victoria Mill was completely rebuilt.</p> <p>Flourmill was listed at the address 7-11 Keppel Street and a residence (8 rooms) at 13 Keppel Street. The annual value of the mill was recorded as 222 pounds and the residence as 40 pounds. From 1914, Eliza Davis Tremain is listed as the owner of the residence.</p> <p>Edward Williams expanded the drapery and occupied both the shops at 3 and 5 Keppel Street. The two shops are recorded as having 9 rooms each. The annual value of the shop is recorded as 208 pounds. The drapery measured 53' x 66' at this time.</p>
1926-28	<p>A bookshop and grocers are listed along with the Flourmill. The brick residence at 13 Keppel Street was rented out to the Railway Inspector.</p>
1929-33	<p>Painters and grocers are listed along with the Flourmill. The brick residence at 13 Keppel Street was rented out to the Railway Inspector.</p> <p>Brick Mills and Sheds occupied lots 9, 10 and 11. The measurements of buildings were 18'x 330'; 52' x 264'; 27' x 330'; and 50' x 60' to Havannah Street.</p> <p>Residence at 13 Keppel Street had 8 rooms and measured 39' x 330'.</p> <p>The two shops had 9 rooms each or 5 rooms with Kitchen, Bedroom and Living. One shop measured 30' x 66' and the other 23' x 66'.</p>

2.7 History of Victoria Mill / Tremain's Mill Site

Victoria Mill at the time of purchase in 1874 was a small mill run on steam with millstones used for grinding flour (these stones lie beneath the present dock of the mill). Victoria Steam Mills was one of the five mills located in this part of Bathurst. The other mills included the H.C Mathews at the corner of George and Durham Streets; Boylsons at the rear of Western Stores Ltd, William Street; Britannia, in William Street, opposite St. Mary's School; and Crealy's in Lambert Street adjoining the Railway Hotel. This part of South Bathurst was referred to as Milltown due to a large number of mills and a large number of mill workers' residences that were located here. Milltown is indicated on the 1939 map of the City of Bathurst (refer Figure 10).

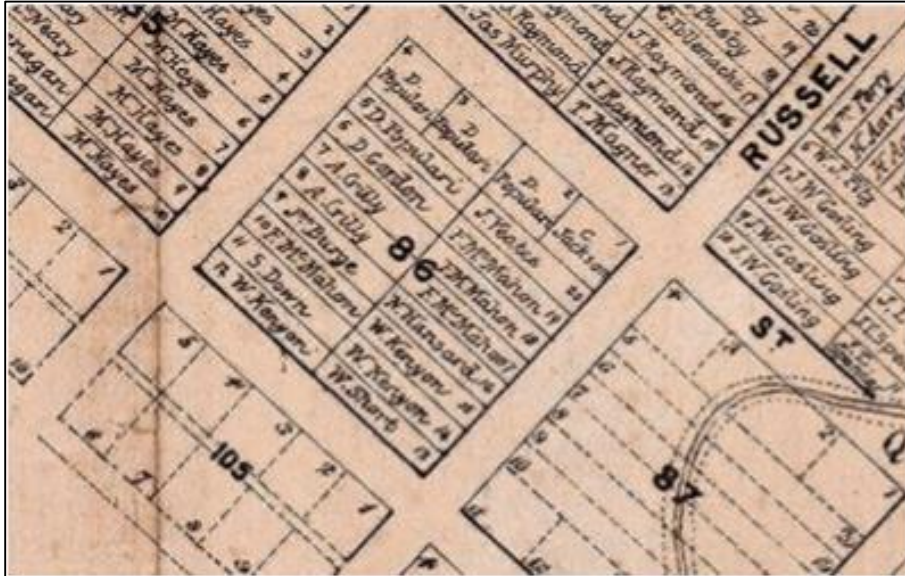


Figure 9: 1860 map of Bathurst.
NSW LPI

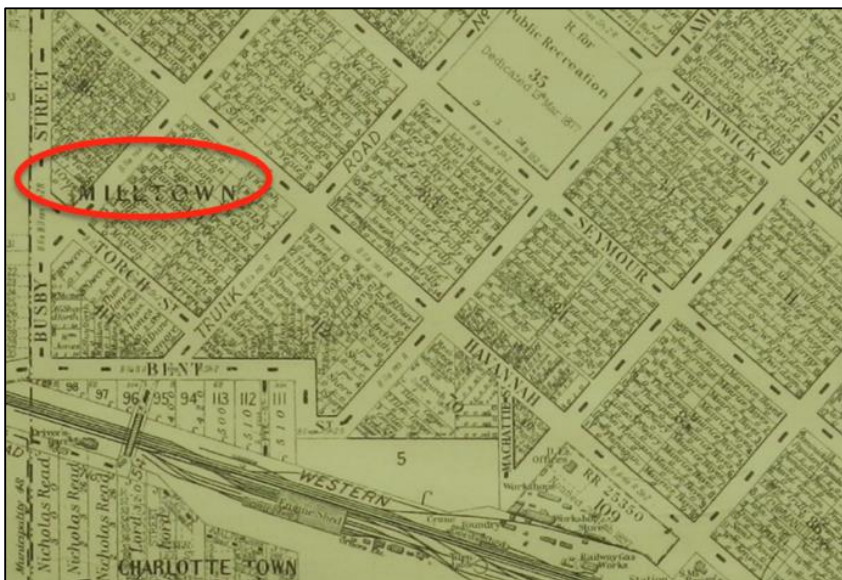


Figure 10: Map of Bathurst, 1939.
Bathurst District Historical Society Archives

The historical development of the Mill complex is illustrated by providing an account for each building that existed on the mill site.

Residence at 13 Keppel Street

A brick residence has been recorded on the site since the time of the sale of Victoria Mills. The sale advertised a brick cottage of 4 rooms, 2 room kitchen and other outhouses. While it is not possible to verify the location of this cottage, it is highly likely that this brick residence sold along with the mill was indeed the Keppel Street cottage at No.13 Keppel Street. In the years 1926–33, the rate records indicate that the brick residence at 13 Keppel Street was being rented out to the Railway Inspector. No rate records are available after 1933.

This residence was replaced with a brick residence later. The council building approval records indicate that a detached brick cottage with six rooms including a kitchen was approved in September 23 1936. The brick cottage that existed prior to the preschool was therefore possibly built in 1936.

The Keppel Street precinct study states that:

The site was originally part of the Victoria Mill complex and occupied by a residence owned by one of the Tremain family. The property included servant's quarters and a cellar and originally stretched to the rail feeder line but was shortened to make way for a weighbridge for mill vehicles. The house was demolished in the late 1960s. Preschool was built in the late 1980s.



Figure 11: No. 13 Keppel Street, 1940.
Tremain Family Archives

Victoria Stores – 3, 5 Keppel Street

In the years 1881–84, in addition to the mill and the residence, the property also included a brick shop. Figure 12 illustrates the single storied shop along Keppel Street with a double storied section at the rear. In 1885–86 an additional shop was recorded in the rate books. The two shops had seven rooms each. At this time, Reuben Tremain, the son of William Tremain occupied No.5 Keppel Street, which is recorded as a general store. Edward Williams, a draper rented the shop at No.3 Keppel Street.

The Bathurst Municipal Council granted permission to add a balcony along the Keppel Street façade in 1884 (Figure 13). Figure 15 and Figure 17 show the double storied rear section of the building in the 1900s.

The rate records indicate that Reuben Tremain occupied a shop and a dwelling. It is possible that the upper floor to the shop was added at this time. It is also possible that the dwelling recorded was most likely above the shop (Figure 13). From 1887– 1898, the rate records list

Edward Williams as a Draper, who rented the premises at 3 Keppel Street. The shop is listed with nine rooms. A painted sign in the ground floor beam in the present day, illustrates the use of this shop as a drapery in the early 1900s. The rate records indicate that Edward Williams, the draper occupied this shop until 1925. After 1925, the rate records indicate that the shops were used as a bookshop and grocers (1926–28). In the years 1928–33, the rate records list that painters and grocers occupied the shops.

Sometime after 1964, a part of the first floor section at the rear of the Victoria Stores building was demolished. The last two windows seen in the photographs illustrated above were removed. The balcony was also demolished sometime after 1964.

In 1971, the Tremain brothers brought together quality control testing equipment into a modern laboratory. The equipment included a colour grader, a dough testing Alveograph, and nitrogen testing unit to maintain constant control over various types of flour. Hourly tests were taken to ensure that the products met the standards. The laboratory was housed in the upper level of the Victoria Stores building in the 1970s.

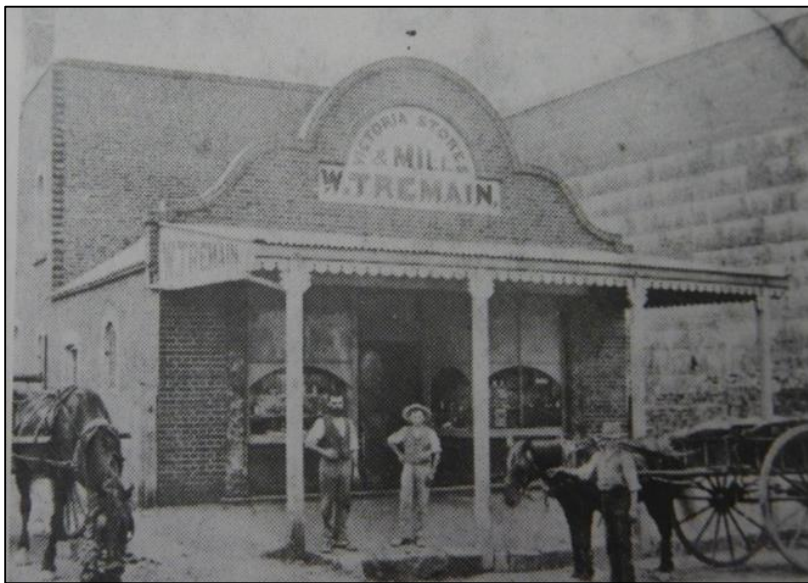


Figure 12: Victoria Stores, 1881.
Tremains of Bathurst, p. 4



Figure 13: Victoria Stores building, 1900.
Tremain Family Archives



Figure 14: Wheat wagon.
Callan Family



Figure 15: Victoria Stores Building, 1940.
Tremain Family Archives

Victoria Roller Flour Mill – later Tremain Bros Pty Ltd

The Victoria Mills at the time of purchase was a humble three storied building with a basement and a square chimney at the rear. Figure 16 illustrates the mill at the time of purchase in 1874. At the time of purchase, the property included the steam flour mill building with a boiler and a brick cottage with four rooms, two room kitchen and other outhouses. The railways station was being proposed opposite the mill.

In the late 1800s, as new wheat lands were opened up in the west, it was realised that Bathurst district was not a successful wheatgrowing district. As the railway extended to the west, the Bathurst mills were affected. In order to compensate the mills left behind, the NSW Railways introduced a system of rail rebate called 'Milling in Transit'. This rebate and the stabilisation of the milling industry helped the Tremains to modernise the plant and develop their business. William Tremain and his family worked hard and were soon able to establish Victoria Mills as one of the major mills in the region.

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Figure 16: The original Victoria Mills, 1874.
Tremain Family Archives

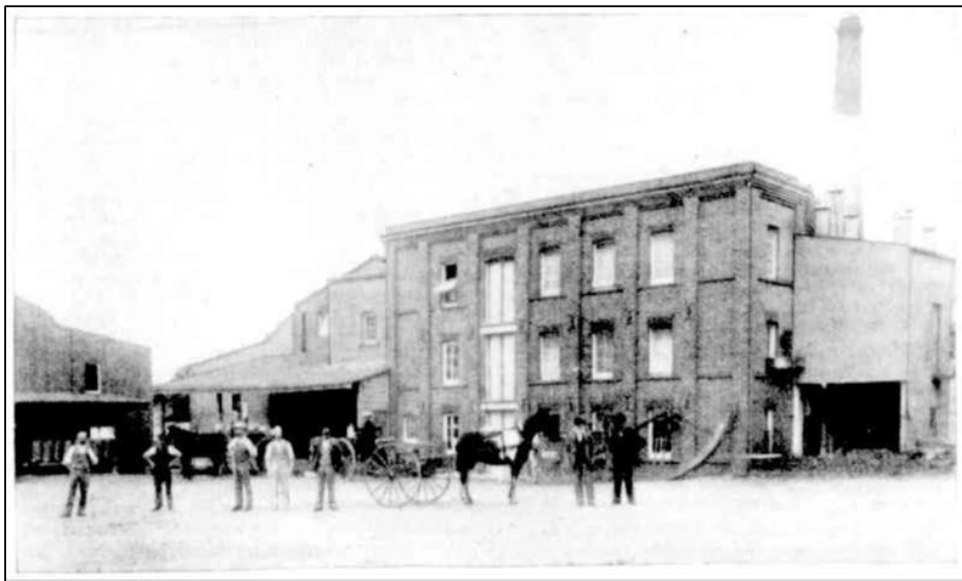


Figure 17: Victoria Mills, 1896.
Town and Country Journal, November 21 1896

An advertisement in the 1880s by the leading bakers in Sydney and surrounds provided a testimonial to the quality of flour produced by a small number of mills in NSW of which Tremain's is listed. W. Tremain installed new equipment in the mill, which improved the quality of their product, and they won a number of prizes.

Advertisements in the local newspapers became a regular feature in the 1880s. In 1895, the old millstones were replaced with steel rollers and cleaning, grading and dressing machines were installed.

The following maps of Bathurst illustrate the Victoria Mills site during the years 1882 and 1897. The railway line is clearly visible and it ran very close to the mill site.



Figure 18: Map of Bathurst, 1882.
NSW LPI



Figure 19: Map of Bathurst, 1897.
NSW LPI

The Tremain's Mill flour won a medal from Calcutta International Exhibition in 1883–84; from the Chicago Exposition in 1892–93; and twelve first and special prizes in Agricultural Shows at Bathurst. Owing to the success, Tremain took on another mill in Bathurst, known as the Britannia Mill in 1889.

Britannia Mill was centrally placed on the block surrounded by Piper, Bentinck, Keppel and William Streets. Although Tremain purchased Britannia Mills in 1889, it was only made operational in 1899.

Owing to the success of the roller flour, the mill was named as the Victoria Roller Flour Mills and the Tremain's 'Invincible' patented roller flour became very popular and was a common household name. Figure 17 illustrates Victoria Mills in 1896. Regular ads highlighted the use of Henry Simon's rollers that were installed to improve Victoria mill's performance (Figure 21). The quality of flour turned out superior in fineness and colour and this prompted Tremain to adopt a brand 'Invincible'. The Tremain's marketed their flour as 'Invincible' and 'Fairy Dell'. Both Tremain's Flour Mill and Crago Flour Mill used calico flour bags for many years (Figure 19 and Figure 20).



Figure 20: Advertisement.

Bathurst Free Press and Mining Journal, 24 January 1885.



Figure 21: Advertisement.

Bathurst Free Press and Mining Journal, 29 March 1889.

Tremain's Mill was finally established on the road to success. An article in the National Advocate in 1890 provided a detailed account of the Victoria Roller Mill. The mill was a three storied brick building with basement and its capacity was 15cwt per hour. A large granary adjoined the mill with a holding capacity of over 3000 sacks. The extract below from the article provides a comprehensive overview of the milling process:

The process of milling is carried out on the same principles as in all other establishments where the Simon system is in operation. The wheat is first received in a large hopper on the ground floor whence it is carried by elevators to the top storey. There are thirteen sets of elevators used in connections with the flour and four other for wheat, all radiating from iron boots in the basement. At the top of the mill the wheat passes through a zigzag separating machine, which extracts all the straw, chaff and other rubbish. Thence the grain undergoes a further process of cleaning in the smutting machine; after that the separating and scouring machine operates on the grain extracting the shrunken grain and other extraneous matter; from the scouring machine the wheat passes into what is known as the Garden City brush polishing machine, which gives it a beautifully polished appearance; from this the grain is shot into a larger bin on the first set of rollers. Here it receives the first breaking, the grain being properly opened. Thence elevators carry the wheat back to the top of the mill to a quadruple rotary scalping machine which extracts all small particles and passes the wheat onto a second set of rolls. So the operation goes

on till the fourth set of rollers do their work, when the material comes out in the form of semolina, bran and middlings; semolina is a term which came into use with the introduction of the roller system, from it the best flour is made. The purifying machines are next encountered, and when their work is completed the work of reduction on the first set of smooth rollers is commenced. Up to this point the crushing rollers are more or less corrugated. The dust from the purifiers and other cleaning apparatuses is blown by powerful fans into a couple of cyclone receptacles, whence it falls into receiving bags. As the process of reduction goes on, the semolina and middlings pass on to centrifugal dressing machines, which gives the flour its finishing touch. Finally the flour is carried to the top storey and descends a large shoot into the receiving bags. The impetus it receives in the fall dispensing with the necessity of 'ramming' the bags. Everything about the process is automatic, the grain not being handled in any way from the time it is lowered into the receiving hopper until the flour is taken off the bagYfiller ready for sewing up. The main driving belt is 10" wide, and the series of belting which intersperse the several storeys render the peregrinations of one unaccustomed to the places somewhat difficult and dangerous, notwithstanding the admirable means of protection provided by Mr. Tremain. The plant is driven by a WelshYmade engine on the 'beam' principle. Its nominal capacity is 25 h.p., and the engine has been in active work for over 30 years. Of course numerous alterations have had to be made; amongst them a new cylinder has been put in, and some of the bearings have been replaced, but on the whole the engine despites its oldYfashioned appearance, does its work just as effectively as those of modern make. The boiler is 30' in length, 6' in diameter, with five Galloway tubes, 27' long.

However, the good times did not last long. William Tremain, the founder died in 1898. He had served as the alderman for Bathurst Council from 1882–99. According to the family's published history, William Tremain had served on the Bathurst Council in 1895, 1896 and 1897. Soon after, a devastating fire destroyed the mill in 1901.



Figure 22: Invincible calico bag.
Bruce Tremain



Figure 23: Fairy Dell calico bag.
Bruce Tremain

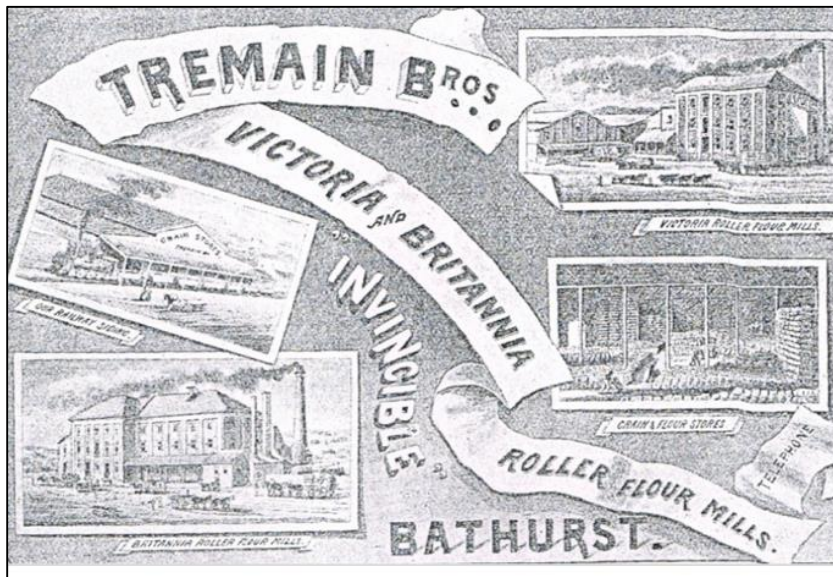


Figure 24: Listed under Trades.
Bathurst Federal Directory and Illustrated Gazetteer, 1900.

The advertisement illustrated in Figure 24, featured in the 1900 Bathurst Federal Directory provides images of the Victoria and Britannia Mills, Grain and flour storage at the time.

On 21st July 1901, a devastating fire destroyed most of the Tremain's Victoria Mills causing £5000 worth of damage. Probably it was fortunate that the Tremain family owned another flour mill. Britannia Mills had been set up in 1899 with the latest and improved machines and new offices. The newly installed Simon's machines ensured the milling of fine flour. As Britannia Mills had recently been set up, the milling operations were shifted, allowing Victoria Mills to be rebuilt.



Figure 25: Victoria Mills, 1901.

Tremain Family Archives

Within five months after Victoria Mills were destroyed, the entire mill was rebuilt and fitted with improved machinery. A news article in the Bathurst Free Press and Mining Journal provides a detailed account of the rebuilt Mill in 1902.

According to the article, Victoria Mill was one of the oldest established in the west, but had been improved from time to time until the building was completely gutted in the fire of 1901. After the fire, the only portions of the main building standing were the main walls. The Tremaims took advantage of the opportunity to build a model mill. The remnant building destroyed in the fire was completely demolished with the exception of the front wall. The large wheat store adjoining the building was not damaged, and this was retained with the additions made to the shed in the form of an elevator, which extends to the roof and will convey the wheat to the silos and from the silos to the grain cleaning machinery.

The following excerpt provides a comprehensive overview of the rebuilt model mill:

The walls of the mill proper had been considerably raised so that more room is given on each floor. On the basement 8 double sets of roller mills with the latest contrivances, have been erected. The arrangements are so complete and compact that the floor is entirely free from dust. The second storey is known as the Purifying Floor and on this several of Simon's latest improved purifiers and centrifugal scalpings are at work, while on the third or Centrifugal Floor, a cyclone dust collector has been built. Into this the hot air dust is carried off the rolls and exhausted by means of powerful fans, while the spent air is passed through pipes leading out of the roof. The dust room has been dispensed with and each of the cleaning machines is connected with the cyclone dust collector. By this means the danger of fire is reduced to a minimum and everything is exceptionally clear and free from dust. The Centrifugal and Wheat Dressing Machines occupy the top floor.

The wheat dressing department is separated from the milling and here, on the top floor is the smutter and also the polishing and scouring machines, while on the second floor, three oats and barley cylinders are in full work. These machines extract the oats and barley, purifying the wheat before passing on to the rollers. On the basement the wheat is emptied into a sieve which removes all rough straw and

other impurities before it is carried into the smutter. The different departments are separated by strong iron doors, so that in the event of fire there is less danger of damage and the flames can be more easily located.

The chimneystack was pulled down and rebuilt. It now reaches to a height of 72' and is strong and substantial. The engine is one of the most compact of its kind in the State and increased capacity has been secured. The mill is now turning out 10 sacks per hour and in order to make up for lost time, the mill is working night and day. The electric light has been installed and altogether the proprietors have occasion to be proud of the property.

The mill was therefore successfully rebuilt and its efficiency and productivity increased.



Figure 26: Victoria Mills, 1902.
Tremain Family Archives



Figure 27: Sewer diagram of site.
Bathurst City Council

The sewer diagram above provides an understanding of the Tremain Flour Mills in 1917. The buildings clearly visible are the brick mill, smaller and larger shed, residence along Keppel Street with a kitchen behind and outhouses and the Victoria Stores.

Figure 28 illustrates the mill in 1940s. The original brick building is clearly evident with its openings. The timber silos have been added at this stage and can be seen behind the brick portion of the building. The Oregon timber grain elevators are discussed later in this chapter. The octagonal chimney with two different coloured bricks is visible.



Figure 28: Flour Mills, 1940.
Tremain Family Archives

In 1957, additions were made to the mill. An entire floor was added along with a staircase to access the building. The changes are clearly evident in Figure 28 above.

The old octagonal chimney was designed with cream coloured corner bricks to complement the main red bricks. However, after electricity was installed, the boilers, steam engine and the chimney were no longer required. In May 1972, the chimney was demolished.

The sign Invincible Flour was created along the footpath on Keppel Street in front of the mill site. The sign was possibly installed towards middle of twentieth century.



Figure 29: Flour Mill, 1970.

Tremain Family Archives

Large Shed adjacent to Brick Mill Building

A large shed was adjacent to the mill in the late 1800s. At the time of the fire, this shed was not damaged (Figure 30). The brick shed has a brick gable wall with a gable iron roof and two skillion roofs on either side at a slightly lower height. A timber truss on undressed timber columns supports the roof. Undressed timber columns were a common practice in the early twentieth century in Australia, often termed bush architecture.

The building in 1900 had an awning in the front as illustrated in Figure 30. This large shed had a capacity to hold 22,000 sacks and this was nearly full of wheat in 1902. The brick gable wall has brick pilasters along the outside. The windows have segmental arches with bricks placed on the edge.

The sewer diagram in Figure 27 indicates that a larger shed and a smaller shed were present on site at this time. In 1917, a building approval request was made for a shed on Keppel Street, estimated at 130 pounds. It is highly possible that this was the extension to the shed building at this time. Extension to the shed on either side was made beyond the skillion roofs.

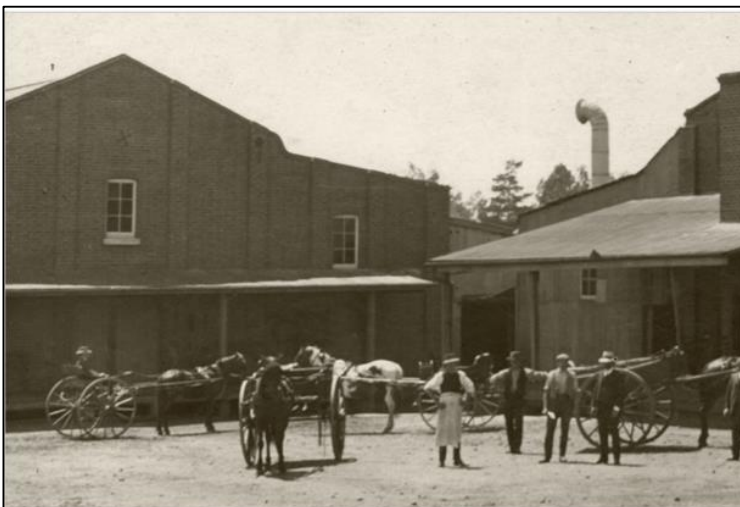


Figure 30: Shed building, 1902.

Tremain Family Archives

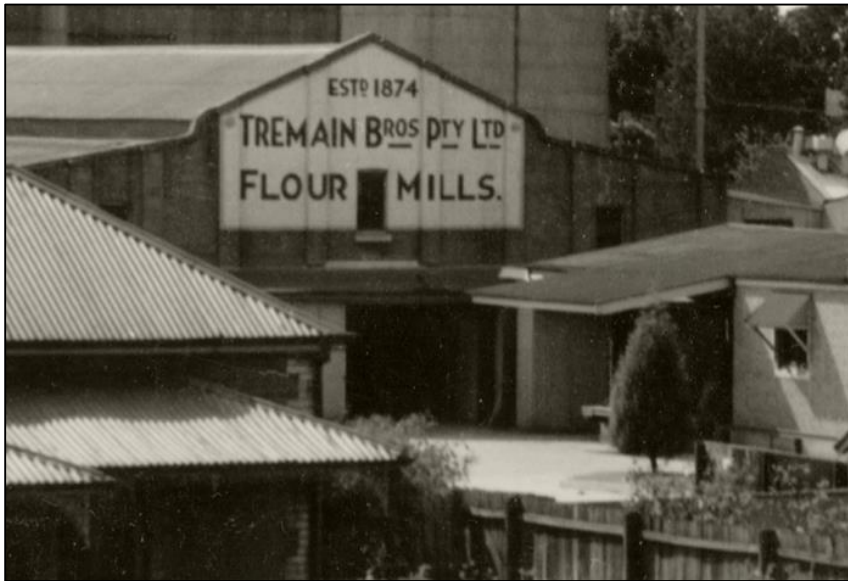


Figure 31: Shed building, 1940.
Tremain Family Archives

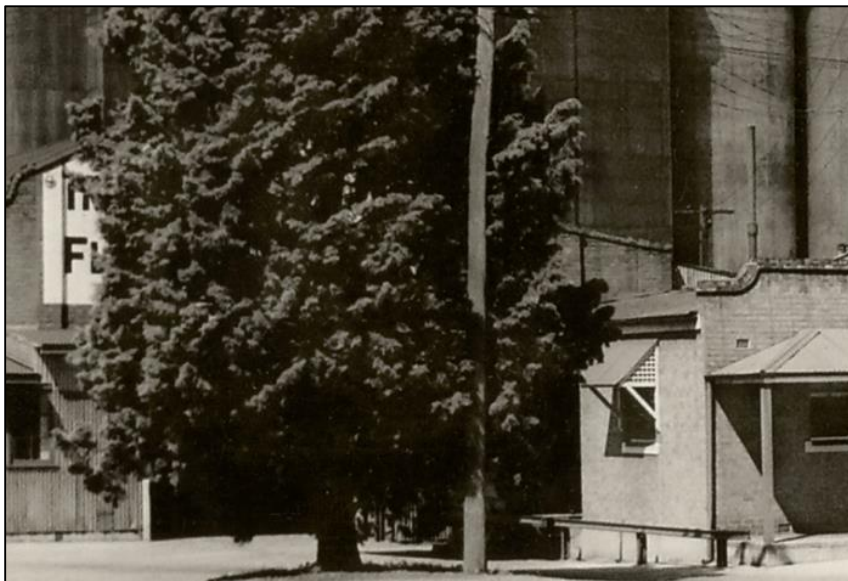


Figure 32: Shed building, 1970.
Tremain Family Archives

Figure 32 illustrates a view of the shed in 1970. Although a large tree is blocking the shed, the Tremain painted sign is clearly evident.

The front awning was removed at a later date and the central window blocked in. A weatherboard structure was added to the front, which was probably higher than the awning.

A smaller shed to the northwest of this large shed was constructed at the same time as this large shed (Figure 27). The smaller shed also displays the same construction details as this large shed. It has undressed timber posts as columns that support a timber truss and the roof. Figure 34 to Figure 37 illustrate this shed at the time of the construction of the Oregon timber silos.

Tremains constantly upgraded the milling equipment and kept the mill up to date in technology. The electric bag elevator was purchased to bag the flour and can be seen in Figure 33. The machine, nicknamed as the Dinosaur, is photographed with the mill staff in the

large shed. The undressed timber columns of the large shed are clearly visible in the background.



Figure 33: Electric bag elevator, 1918.
Tremain Family Archives



Figure 34: Large shed (right) and smaller shed (left), 1932.
Tremain Family Archives



Figure 35: Large shed (right) and smaller shed (left), 1932.
Tremain Family Archives



Figure 36: Large shed (right) and smaller shed (left), 1932.
Tremain Family Archives



Figure 37: Large shed (right) and smaller shed (left), 1932.

Tremain Family Archives

As illustrated in the figures above, the large shed and smaller shed are visible at the time of the timber silo construction. Figure 37 further illustrates that there was a timber fence surrounding the property along Manilla Street in 1932. A gate is also visible in the photograph.

Railway Siding

Prior to his demise, William Tremain had entered into an arrangement with the Railways to build a connecting line to the Victoria Mill at his own expense. Rail trucks at that time had a capacity of only eight tonnes and they were shuttled to and from the mill by horse. Steam engines replaced the horses later. A railway siding was added which connected the mill with the railway line (Figure 38, Figure 39).

A weighbridge capable of carrying 20 tonnes is located along the railway line. This allowed in a considerable saving of time and labour in handling the wheat and flour.



Figure 38: Map of Bathurst, 1922.

NSW LPI

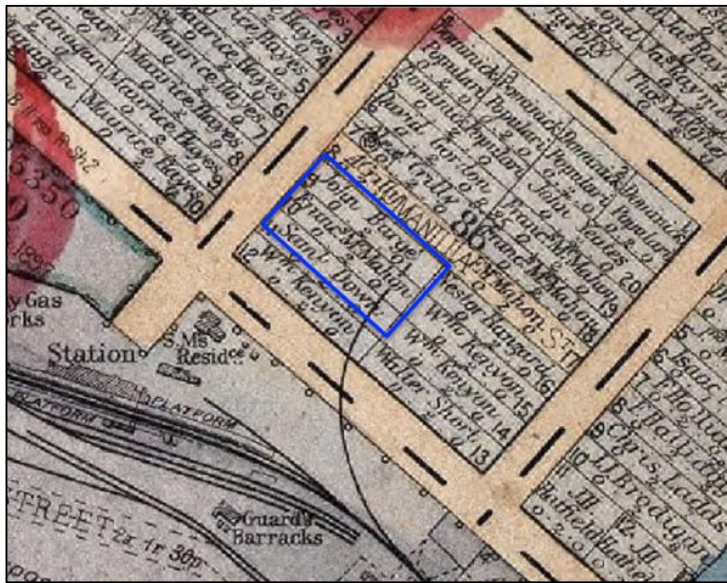


Figure 39: Map of Bathurst, 1927.
NSW LPI

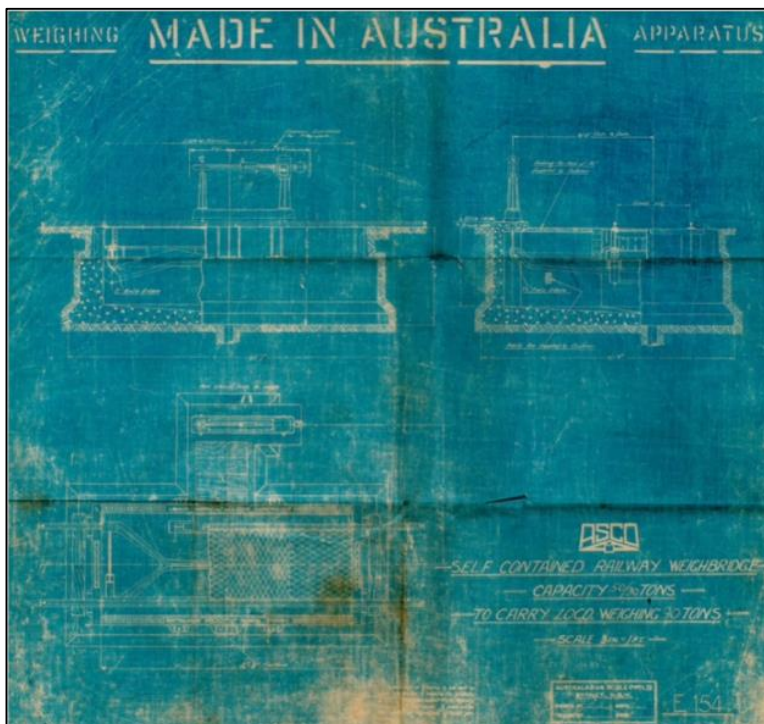


Figure 40: Railway weighbridge drawings, 1927.
Tremain Family Archives

Management of the Mills

Following the death of W. Tremain, the running of the mill and other business interests came under the management of his three sons, the eldest Reuben, John Thomas Westlake and Edwin Peter. John and Edwin took over the managing of the mill. Reuben Tremain managed the Victoria stores until 1895, where some of the flour and by products from the mill were sold. After 1895, the store managed by Reuben Tremain as a grocery was rented out to other businesses.

Following the reconstruction and running full capacity for a short while after the rebuilding, the next harvest failed. This meant that the Tremain's Mill suffered another setback. The Henry Simon machinery from Britannia Mill was transferred to a new mill at Boggabri and the Britannia Mill site was sold. The Tremain's attempts to expand proved futile as the Boggabri Mill was losing money. The times were hard for flouring operations and only two remained in Bathurst: Crago and Tremain's.

The outbreak of World War I and a failure of the wheat crops in 1914–15 caused further problems. Due to the demand of flour for troops and civilians, a state wheat board was formed to implement the Wheat Acquisition Act, which allowed a welcome financial breather for the Tremain's.

Grain Elevators for Flour Mills

In 1916 the Grain Elevators Board Act was passed enabling an efficient system of storing and packing. In 1918 the first American type bag elevators were built by Laycocks of Bathurst and it replaced the cumbersome method of stacking and hand loading of trucks.

Mr John Dunckley of Bathurst built the first grain elevator for Crago's Federal Flour Mills in Newtown. The grain elevator was built with foundations of concrete and posts and girders of ironbark. The bins made of Oregon timber measured 6' x 2', diminishing to 3.5' x 2'. The bins were 47' deep and 10' from the ground with a holding capacity of 60,000 bushels; belt conveyors and bucket elevators helped in quick and cheap handling of grain.

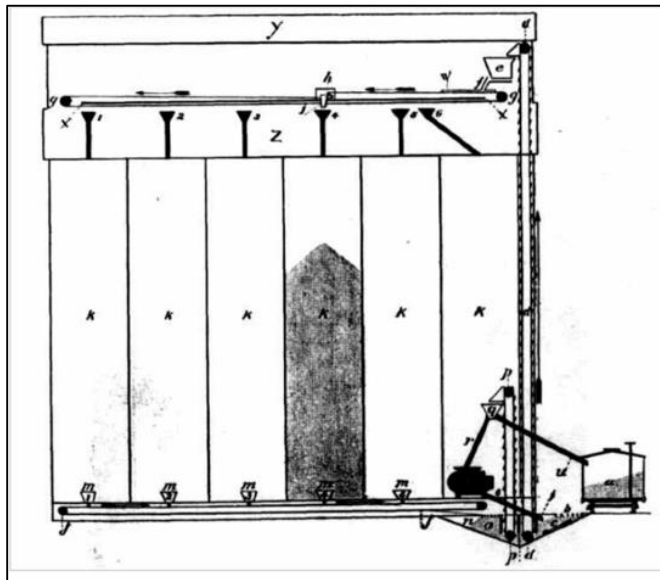


Figure 41: Diagram of a typical grain elevator.
Australian Town and Country Journal, 9 March 1901.

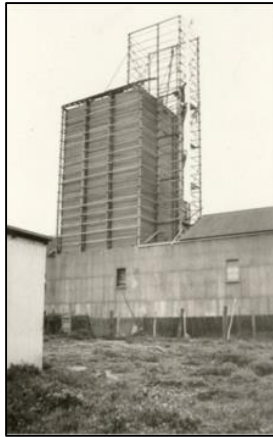


Figure 42: Nine-bin silo, 1932.
Tremain Family Archives



Figure 43: Nine-bin silo, 1932.
Tremain Family Archives



Figure 44: Nine-bin silo, 1932.
Tremain Family Archives



Figure 45: Nine-bin silo, 1932.
Tremain Family Archives

Following the death of John Tremain in 1926, it was decided to float the company and was named Tremain Bros Ltd, subsequently changed to Tremain Bros Pty Ltd. The first directors were Reuben Robert Tremain senior, William Percival Minell and Jack William Tremain. Jack Tremain took over the business.

A small bakery was given financial support in 1928 to promote local business. However, further expansion plans had to be shelved as depression struck. In 1932, there was a reprieve in the financial situations and some long delayed improvements were initiated. The first of these was to erect a silo of nine bins costing £2200, as bulk handling was now a general practice Figure 42 to Figure 45 illustrate the various stages of the construction of the nine bin silo.

Bulk handling of grain was heavily influenced by the methods practised in North America. Representatives were sent to study the construction methods so that they could be adapted to Australian requirements. Of the suitable materials considered for storing grain, wood was concluded as the best material. Oregon pine was considered the most suitable of imported timber.



Figure 46: Nine-bin silo, viewed from Manilla Street, 1932.
Tremain Family Archives

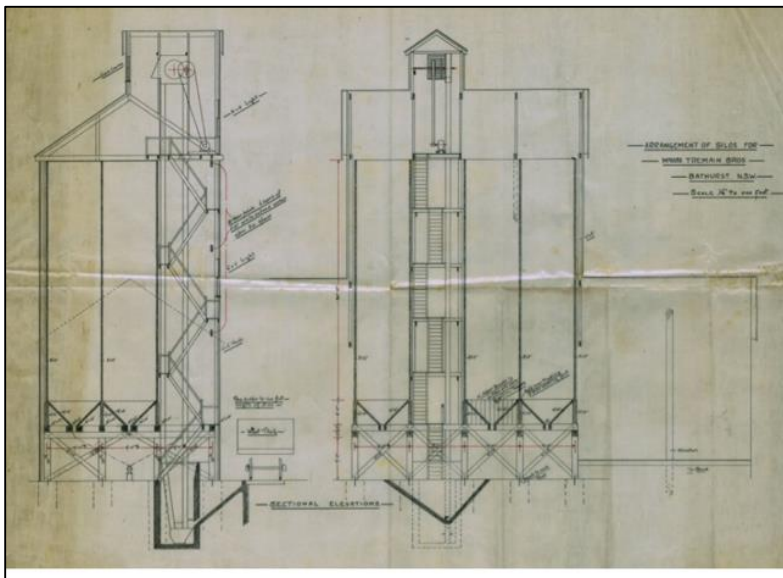


Figure 47: Section drawings for silos, 1932.
Tremain Family Archives



Figure 48: Tremain's Mills, 1940s.

Tremain Family Archives

Further expansion of the Tremain's Flour Mills

Following the construction of the timber silos, the old rice steam engine was replaced with a Bellis Norcom engine of 100 h.p., and some of the plant was reconditioned on modern lines, which included four new roller mills and a Robinson plansifter. The flour mill was now capable of 15/200lb bags an hour. The mill when it went back into production following the modernisation in 1934, the profits increased considerably.

Despite these various measures and improvements, the industry still lacked stability due to the booking requirements and practices that continued to hamper profit for the flour mills. After the formation of the Australian Wheat Board in 1939, the speculation came to an end and with World War II, Bathurst and district contributed fully and effectively.

In 1940, a second set of 90bin silos was erected. The war however curtailed production and provided further opportunity to improve the mill and its plant. The wheat cleaning section was enlarged and redesigned. New plant and bins were installed including three new purifiers, a second Robinson plan sifter which increased the capacity of the mill to 18/200lb bags. In 1942, stock meal production was started and a separate plant was installed for it two years later.

With the end of World War II, Tremain Bros Pty Ltd were ready to expand further. The buildings were extended, more plant and equipment installed and storage and siding accommodation increased. The mill capacity increased to 20/200lb bags. Figure 48 provides a 1948 view of the Tremain's Mill from Keppel Street. The Victorian residence at 13 Keppel Street is visible along with the brick chimney behind the mill building. Victoria Stores building is also longer with five windows at the upper level.

In 1953, capacity was lifted to 23/200lb bags and to cope with the increased production demands, an electric motor of 200 h.p., replaced the steam plant.

In 1954, two concrete silos were added to the mill complex. Figure 49 illustrates the construction drawings for the concrete silos.

The mill building was further expanded in 1957, when an additional floor and a staircase to the southwest were added. Figure 50 and Figure 51 illustrate the construction drawings for the proposed extensions.

Although many mills closed around this time including the Crago Mill in Bathurst, Tremain continued. In 1956 two concrete silos were added and handling equipment rearranged and

mill activities increased due to the production of self raising flour. In 1957 Tremain was the only mill left operating in Bathurst.

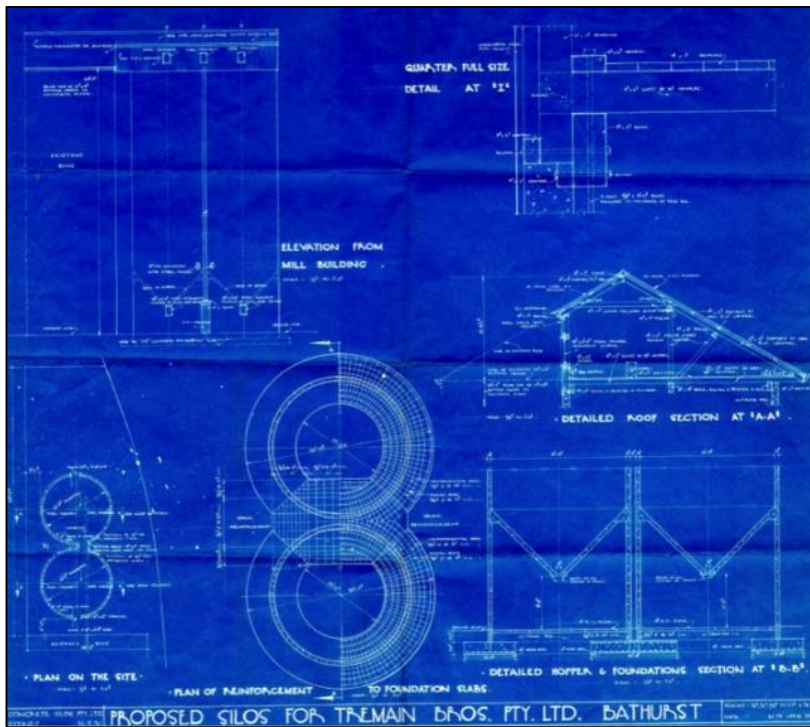


Figure 49: Construction drawings for silos, 1954.
Tremain Family Archives

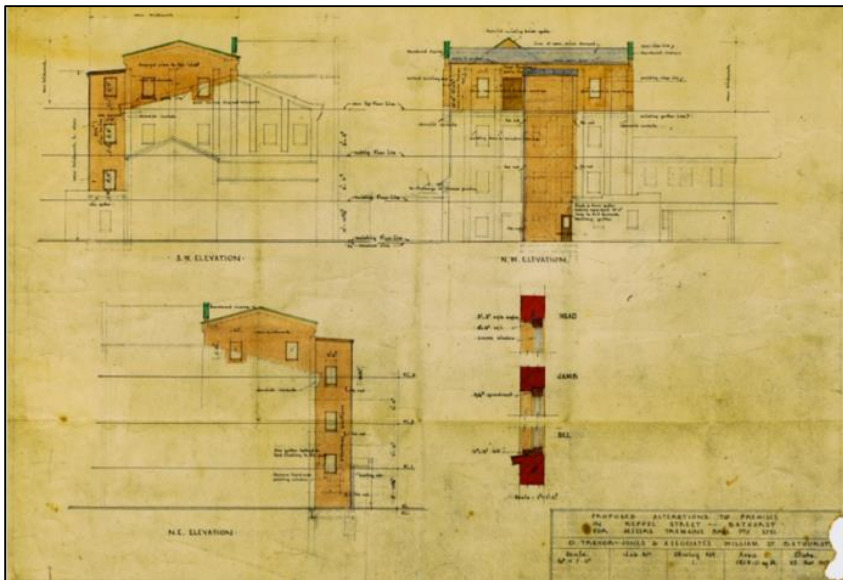


Figure 50: Mill expansion drawings, 1957.
Tremain Family Archives

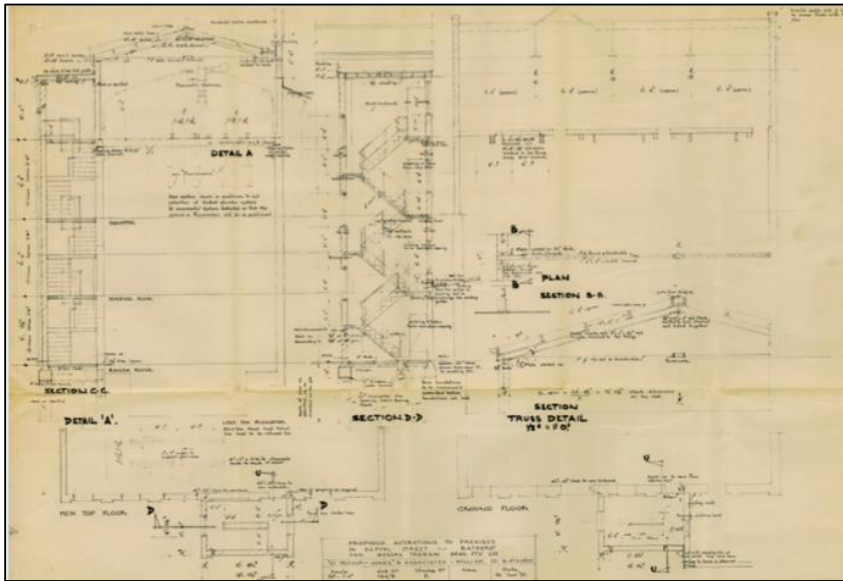


Figure 51: Addition of a staircase to the Tremain's Mill, 1957.
Tremain Family Archives



Figure 52: View of the mill, 1960.
Tremain Family Archives



Figure 53: View of mill, 1970s.

Tremain Family Archives

Brick Store, Havannah Street

A brick store was built along Havannah Street sometime in the early part of the twentieth century. However, since this is not evident in the 1917 sewer drawings (Figure 27), it is highly likely that this brick store was added after this time.

In 1957 a council application was made to add an extension to an existing brick storeroom along Havannah Street at the address 65–67 Havannah Street.

Aerial Imagery

As there are many gaps in understanding the history of this complex industrial site, the available aerial photographs have been examined in order to provide clarity to the historical development of the former Tremain's Flour Mills.

Aerial imagery for the site is available in the latter half of the twentieth century. The earliest aerial imagery on the land and property website is provided in Figure 54 below and it dates back to 1943. Although the image is not clear, it still provides an understanding of the mill site in 1943. The buildings are annotated for reference. The buildings that are clearly visible in the aerial imagery include the residence at 13 Keppel Street, Victoria Stores at 35 Keppel Street, Brick Store along Havannah Street, Mill building prior to extension, addition of timber silos, large shed and smaller shed along Manilla Street. The property of the residence at 13 Keppel Street extends to almost the end of the Bedwells shed.

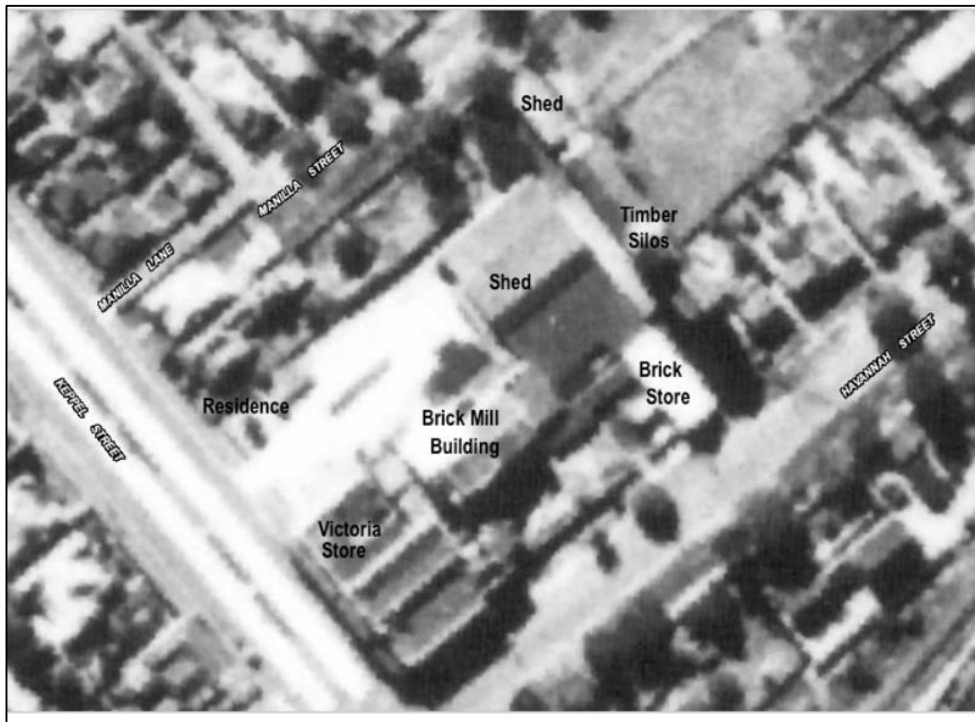


Figure 54: Aerial photograph of Bathurst, 1943.
SIX Maps



Figure 55: Aerial photograph of Bathurst, 1964.
SIX Maps

By 1964 (Figure 55) the number of buildings on the Tremain's Mill Site had grown considerably. The residence at 13 Kepple Street can be seen, however, the backyard of this residence was reduced to allow for the road weighbridge. Victoria Stores building appears at its full extent with the balcony. The mill building is seen with the new floor addition and the

new staircase. The chimney is vaguely visible. The brick store on Havannah Street has been extended.

The land adjacent to the large shed to the southwest is empty except for a small shed. There is a shed to the northeast of the timber silos, but it appears smaller.

The 1983 aerial photograph (Figure 54) clearly illustrates the changes that took place after 1964 and before 1983. The brick chimney, balcony to Victoria Stores along Keppel Street and the Victorian residence at 13 Keppel Street were demolished by this time. A large shed can be seen along Manila Street, clearly built after 1964.



Figure 56: Aerial photograph of Bathurst, 1984.

SIX Maps

Closure of the Tremain's Mill

The Tremain family operated the flour mill for nearly 106 years. To mark the centenary, two publications titled '100 years of milling' and 'Tremains of Bathurst' were published in the 1960s. A scaled model (1:16) of the mill was featured in these publications. There is no current knowledge as to the whereabouts of this timber model of the mill.

After nearly a century of associations with the Tremain's Mill, the Tremain family ended their connections with the Victoria Mill and finally sold the milling rights to George Fielders and Company in 1980. Fielders then closed down the last operating flourmill in Bathurst.

3 SITE ASSESSMENT

3.1 The Site

For the following, refer to Figure 57, an aerial photograph over the site, and to Figure 58, which reproduces the existing site plan.



Figure 57: Aerial photograph of former mill precinct. The subject site is outlined in red. SIX Maps, 2023



Figure 58: Current site plan showing existing buildings. As noted on the plan, buildings shaded green are located within the subject area. Karl Walls Design, 2022

For the purposes of the following description, Keppel Street is considered the western boundary.

The site is irregularly shaped and comprises two allotments, with frontage to Keppel, Havannah and Manilla Streets. The site boundaries are approximately as follows: north and south, 140m; east, 105m; west, 45m. The total site area is approximately 7,440m².

The site contains several built elements of varying scale, described briefly and separately below. For more information, including photographs of the interiors, refer to the *CMP 2016* that accompanies this application.

3.2 Built Elements

3.2.1 Invincible Flour Sign (A)

This is a late 20th century period concrete sign which is located adjacent to the Keppel Street boundary next to the footpath.

Refer to Figure 59.



Figure 59: Invincible Flour Sign on Keppel Street.

3.2.2 Victoria Stores Building (B)

The Victoria Stores Building is located on the southwest section of the site. It comprises a two-storey building in the Victorian Italianate Style, constructed of face brick with a parapet roof clad in corrugated iron. A verandah with a decorative iron lace balustrade and a separate metal roof supported by timber posts extends across the first-floor of the front elevation. The ground-floor has two shopfronts with timber panelled doors and glazed openings. Window openings are typically timber-framed double hung sash windows in arched openings.

Refer to Figure 60.



Figure 60: Victoria Stores Building as viewed from Keppel Street.

3.2.3 The Mill Building (C)

This building is towards the middle of the site. It comprises a three-storey brick building constructed c. 1902 with a gabled roof clad in corrugated iron. There is a corrugated iron structure, a later accretion, on the east side of the roof, visible from Havannah Street. The Tremain's Mill logo is emblazoned on the Havannah Street elevation.

Refer to Figures 61 and 62.

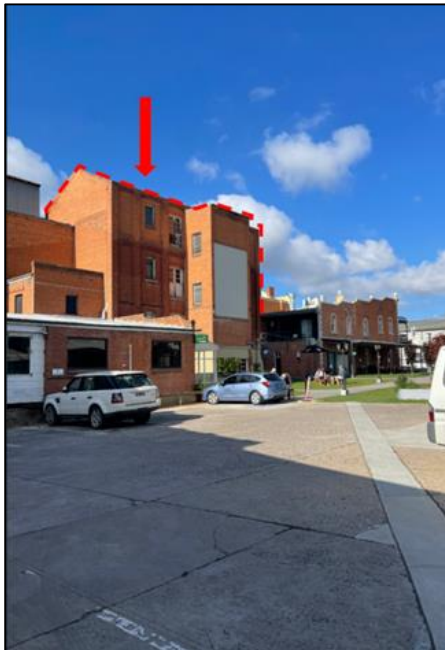


Figure 61: The Mill Building viewed within the site.



Figure 62: The rear of the Mill Building viewed from Havannah Street. The corrugated iron structure on the roof is visible above the Tremain logo.

3.2.4 Bedwells Feed Barn Warehouse (D)

The Bedwells Feed Barn Warehouse is located towards the middle of the site. It comprises a single-storey c. 1881 brick shed with a gabled roof clad in corrugated iron. The elevation addressing Keppel Street has a painted sign of the Bedwells business along with their logo.

Refer to Figure 63.



Figure 63: Bedwells Feed Barn Warehouse as viewed within the site.

3.2.5 Building 11 (E)

This shed is located towards the northeast section of the site. It comprises a single-storey 20th century period iron shed with a gabled roof clad in corrugated iron.

Refer to Figure 64.



Figure 64: Building 11 viewed from Manilla Street.

3.2.6 Timber Silos (F)

This structure is located on the southeast section of the site. It comprises a c. 1932 nine-bin silo constructed of Oregon pine.

Refer to Figure 65.



Figure 65: Timber silos as viewed from Havannah Street.

3.2.7 Concrete Silos (G)

This structure is located on the southeast section of the site. It comprises a c. 1953 concrete silos with an iron roof over the timber bins which extends to cover the silos.

Refer to Figure 66.



Figure 66: Concrete silos as viewed from within the site.

3.2.8 Doppio's Café Container (H)

This is a contemporary shipping container used as a café and is located on the west side of the site near Keppel Street.

Refer to Figure 67.

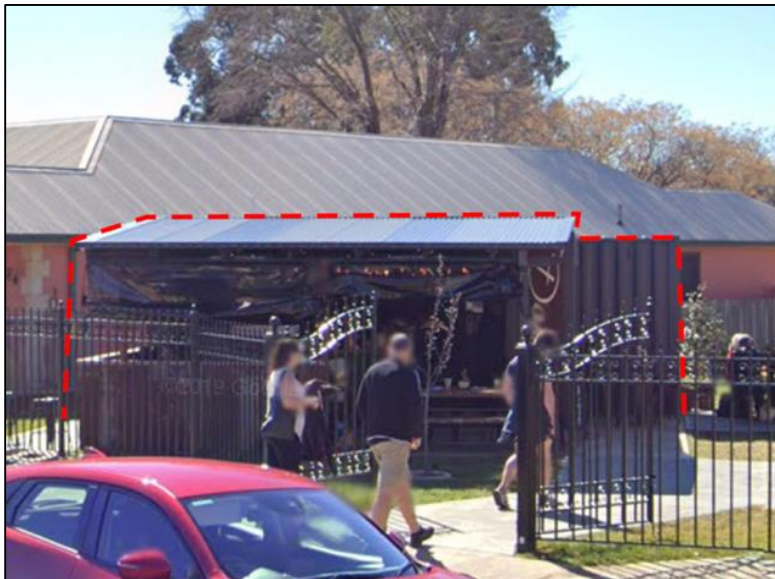


Figure 67: Doppio's Café Container as viewed from outside on Keppel Street.

3.2.9 Fusion Hair Container (I)

This is a contemporary shipping container used as a hair salon and is located on the west side of the site near Keppel Street.

Refer to Figure 68.



Figure 68: Fusion Hair Container as viewed from outside on Keppel Street.

3.2.10 Amenities Block (J)

This is a contemporary element that replaced the Bathurst Rewinds Shed in the same location. It is constructed of brick and has a skillion roof clad in metal.

Refer to Figure 69.



Figure 69: The Amenities Block as viewed within the site.

3.2.11 The Mill Office (K)

This structure is attached to the ground-floor north elevation of the Mill Building. It comprises a single-storey shop constructed of brick, weatherboard and fibre cement with a decorative parapet roof clad in corrugated metal. The window openings are typically timber or metal-framed.

Refer to Figure 70.



Figure 70: The Mill Office as viewed within the site.

3.2.12 Manilla Street Shed (L)

This structure is located on the north side of the site and comprises a mid-20th century period iron shed. Window openings are metal-framed.

Refer to Figure 71.



Figure 71: Manilla Street Shed as viewed from Manilla Street.

3.2.13 Weighbridge and Siding (including industrial artefacts) (M)

This structure is located on the eastern side of the site. It comprises a single-storey iron shed with remnant railway lines adjacent. The shed retains its original equipment including a capstan.

Refer to Figure 72.



Figure 72: Weighbridge and Siding as viewed from within the site.

3.2.14 Bedwells Rear Warehouse (N)

This structure is located on the north side of the site. It comprises a single-storey iron shed constructed towards the end of the 20th century.

Refer to Figure 73.



Figure 73: Bedwells Rear Warehouse as viewed from Manilla Street.

3.2.15 Building 16 (O)

This structure is located on the east side of the site. It comprises a single-storey c. 1917/c. 1957 brick building with a double gabled roof clad in corrugated iron.

Refer to Figure 74.



Figure 74: Building 16 as viewed from Havannah Street.

3.2.16 Garages (P)

These comprise single-storey metal structures with metal roofs located on the north side of the site near Manilla Street.

Refer to Figure 75.



Figure 75: The garages as viewed from within the site near Manilla Street.

3.3 The Surrounding Area

3.3.1 The General Area

For the following, refer to Figure 76, an aerial photograph of the site and its surrounds.

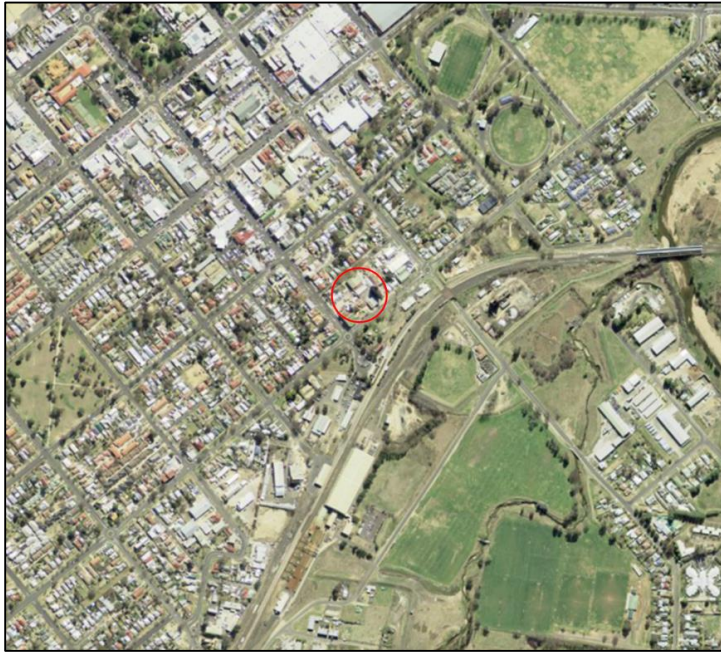


Figure 76: Aerial photograph of surrounding area. The site is outlined in red.

SIX Maps, 2023

3.3.2 Keppel Street

Keppel Street runs north-south between Havannah Street and Edgell Street. The road carries two-way traffic with streetlights on a median strip that extends to George Street. There is provision for street parking on both sides, as well as concrete footpaths. The section of Keppel Street in which the site is located is mixed residential and commercial and characterised by single-storey Victorian and Federation and Inter-War period cottages, two-storey Victorian period terrace rows and two-storey Victorian period commercial buildings. There is later 20th century period commercial infill located to the north on Keppel Street, away from the site.

To the south of the site is No. 3 Keppel Street, a two-storey Victorian period hotel building. To the north is No. 13 Keppel Street, a single-storey late 20th century period building used as a childcare centre. Further north is a group of four single-storey Federation period cottages numbering 15-21 Keppel Street. Opposite the site, to the west, are Nos. 12 and 14 Keppel Street, a single-storey Federation period cottage and a single-storey Inter-War period dwelling respectively. In addition, there is also Nos. 2 and 2A Keppel Street which are both two-storey Victorian period residential dwellings.

Refer to Figures 77 to 80 which illustrate the streetscape.



Figure 77: No. 3 Keppel Street to the south of the site.



Figure 78: No. 13 Keppel Street adjacent to the north of the site.



Figure 79: An example of single-storey Federation period cottages to the north of the site.



Figure 80: Looking towards Nos. 2 and 2A Keppel Street to the west of the site, on the opposite side of Keppel Street.

3.3.3 Havannah Street

Havannah Street runs east-west between Panorama Avenue and Great Western Highway. The road carries two-way traffic with provision for street parking on both sides, as well as concrete footpaths with established streets. The section of Havannah Street in which the site is located is mixed residential and commercial. The Bathurst Railway Station, a late Victorian period building and railway line, is located directly opposite the site on the south side of Havannah Street. To the west of the site, on the north side of Havannah Street, are groups of two-storey Victorian period terrace rows. To the east, on the same side of the street, are 20th century period commercial buildings, as well as single-storey Victorian period cottages.

Refer to Figures 81 to 84 which illustrate the streetscape.



Figure 81: Bathurst Railway Station located on the south side of Havannah Street.



Figure 82: An example of Victorian period cottages to the east of the site on the north side of Havannah Street.



Figure 83: An example of 20th century period commercial buildings on the south side of Havannah Street, to the east of the site.



Figure 84: An example of 20th century period commercial buildings on the north side of Havannah Street, to the east of the site.

3.3.4 Manilla Street

Manilla Street runs east-west between Manilla Lane and Russell Street. The road carries two-way traffic with provision for street parking on both sides, as well as concrete footpaths and grass verges with established trees. The south side of Manilla Street is largely occupied by the former mill precinct. The north side is characterised by single-storey Victorian period dwellings.

Refer to Figure 85 which illustrates the streetscape.



Figure 85: An example of Victorian period cottages directly to the north of the site on Manilla Street.

4 ASSESSMENT OF SIGNIFICANCE

4.1 Summary of Statutory Heritage Listings for the Site

No. 7 Keppel Street and 67 Havannah Street, Bathurst:

- Is listed as a local heritage item by Schedule 5, Part 1 of the *Bathurst RLEP 2014* (Tremain's Mill & Silos (formerly Victoria Mill) & Invincible Flour Sign').
- Is located in the 'Bathurst Heritage Conservation Area by Schedule 5 Part 2 of the *Bathurst RLEP 2014*.
- Is not listed as an item on the State Heritage Register under the *NSW Heritage Act 1977*.

4.1.1 Statement of Significance for the Site

The *CMP 2016* provides the following Statement of Significance for the former Tremain's Flour Mill:

The twentieth century flourmill provides tangible evidence of the role of the wheat industry in the economic development of Bathurst, Australia's oldest inland town. Wheat industry played an important role in Australian economy in the late nineteenth and the early to mid twentieth century. Bathurst was one of the centres revived through the 'Milling in Transit' rail rebate scheme introduced by the NSW Railways to sustain the flour mills left behind when wheat production shifted to central northwest NSW. Tremain's Flour Mill continued milling the flour in Bathurst for over a century.

The mill buildings and the silos are a key landmark in this part of Bathurst. They are an important marker in the historical timeline of Bathurst and in establishing the importance of wheat growing and processing through milling. In addition, they are also important to understanding the role the mills played in boosting the economy of the Bathurst regional district and the NSW state. Although the mill

has been constantly upgraded and the flour milling processes subject to quality and development, the mill complex has remained free from large-scale redevelopment. The mill buildings and the immediate surroundings include the heritage-listed buildings of Victoria hotel, Railway precinct buildings along Keppel and Havannah Streets. This area forms a part of Milltown, due to the presence of mills and mill worker residences. The largely intact surroundings free from large scale modern development contributes significantly to the historic character and setting of the former Tremain's Flour Mills. The mill complex in turn contributes significantly to the Bathurst conservation area.

The earliest mill and shed buildings on the site date from the late nineteenth century, possibly as early as 1880s. The mill site has historical and associational significance due to its association with the founder, William Tremain who served on the Bathurst City Council in the late nineteenth century. The Tremain family ran the flour milling business for over a century. Tremain's trademark 'Invincible and Fairy Dell' flours were very popular and the company won a number of awards for the superior quality of its flour. The mill was successful in a number of international exhibitions and agricultural shows around the country. It is one of the earliest mills in Bathurst and it was the last flour mill to cease milling operations in 1980.

The mill complex has always been associated with residences and shops. In the early days, the owners resided on the site and the mill produce was sold from the shop on premises.

The former Tremain's Flour Mill complex including the brick mill buildings, sheds and associated silos are a common design for industrial buildings of this period. The sheds with undressed timber trunks as columns, brick buildings with exposed brick and simple detailing, Victoria Stores building in a Victorian Italianate style, rail weigh bridge and siding, smaller and larger (30 and 90) tonne Oregon timber grain elevators, concrete silos, additional sheds, and a store along Havannah Street preserve the lay-out of the former flour mill site. The raised inscription on the brick parapet of the Victoria Stores building, the painted signs on the façade of the mill building and the shed, and the 'Invincible Flour' sign along Keppel Street further contribute significantly to the character of the former Tremain's Mill complex. The mill complex, its various buildings, remnant infrastructure, equipment and silos have considerable aesthetic significance.

The Tremain's Flour Mills has considerable social significance at the LOCAL level as established through the advocacy towards its protection prior to its sale in 2015. It is part of an important, long serving local family business in Bathurst and would potentially be of social significance due to its association with a number of its former employees and others connected with the mill.

The buildings, the lay-out, extant machinery, equipment and building details provide an insight into the functioning of the flour mill complex and helps in an understanding of the flour milling techniques and processes practiced in the late nineteenth and early to mid twentieth centuries. Archaeological investigations of sub-surface remains if any, may help reveal early milling practices, residential life and commercial enterprises. The buildings, their inter-relationships, their layout and archaeology have a strong

research potential. Scientific investigations of remnant equipment may help clarify some of the workings of the milling processes in the late nineteenth and twentieth centuries.

The mill buildings and the associated silos are of a generic design from the early to mid twentieth century, but these examples are becoming increasingly rare as mill complexes of this period are gradually demolished or substantially altered. Tremain's Mills is one of the few mills that to retain its earliest shed and original Oregon timber silos. Some original flour milling equipment including the complete grain-handling equipment, roller mill, dreadnought grinder and rail weighbridge equipment are also present *in situ*. The Tremain's Flour Mill is probably one of the few from this period to retain some of its equipment, sheds and silos. Its rarity value is further increased due to the integrity of its setting, its relationship to the railways and surroundings, the town and the Bathurst conservation area.

The former Tremain's Flour Mill at Bathurst is the only remaining tangible evidence of the Tremain family's milling empire and Bathurst's flour mills, and it is one of the second last mills to remain in the town. The mill and its associated buildings display remarkable integrity and are considerably intact, when compared to other mills in NSW.

The former Tremain's Flour Mill is therefore of significant LOCAL heritage significance with historic, associative, aesthetic, social, rare and representative values. The property is relatively intact and has a high degree of integrity. Owing to its integrity and intactness, and the strength of its values, the property may be potentially suited for listing on the NSW State heritage register.¹

This Statement is adopted for the purposes of this assessment.

4.1.2 Statement of Significance for Bathurst Heritage Conservation Area

The *Bathurst RDCP 2014* provides the following Statement of Significance for the Bathurst Heritage Conservation Area:

The conservation area of Bathurst City is historically significant because it includes the commercial and civic heart of Bathurst and reflects the nineteenth century development of this important NSW provincial centre. Bathurst was the first town founded west of the Blue Mountains, and it was a place that developed a high level of prosperity during the latter half of the nineteenth century, particularly as a consequence of the gold rushes of the central west region as well as the pastoral and other industries of the Bathurst district. Additionally, much of the conservation area is of integral social importance for the community because of its lengthy association with various themes in Bathurst's history. Incorporating a wide range of building forms, types and styles, the area has a richness of cultural features. It includes a number of excellent examples of particular architectural styles from the Victorian and Federation eras, and these include Classical, Gothic and Romanesque styles among others. With its historic character, the cohesiveness created by the red brick used in many of the buildings, the landmark

¹ *CMP 2016*, pp. 120-122.

qualities of the spires, domes, massing and other attributes of a number of the buildings, the inherent qualities of the architectural styles represented, and the fine parks and streets, the conservation area is a part of Bathurst that has strong streetscape and other aesthetic qualities.²

This Statement is adopted for the purposes of this assessment.

4.2 Heritage Items within the Vicinity of the Site

For the following, 'within the vicinity' has been determined with reference to physical proximity, existing and potential view corridors and the nature of the proposed works.

4.2.1 NSW Heritage Act 1977

There are two items listed on the State Heritage Register under the *NSW Heritage Act 1977* within the vicinity of the site:

- **'Bathurst Street Lamps', Keppel Street, Bathurst**

This item is located adjacent to the west of the site. It comprises a row of streetlamps in the median strip which extend the length of Keppel Street. The State Heritage curtilage for the item is confined to the base of each lamp. This item is also listed locally by the *Bathurst RLEP 2014*. Refer to Section 4.2.2 for further information on the significance of this item.

- **'Bathurst Railway Station, yard group and movable relics', Havannah Street, Bathurst**

This item is located to the south of the site. It comprises the Bathurst Railway Station, with the curtilage extending to include part of the railway line and yard group. This item is also listed locally by the *Bathurst RLEP 2014*. Refer to Section 4.2.2. for further information.

4.2.2 Bathurst RLEP 2014

Refer to Figure 86, which shows heritage items and conservation areas within the vicinity of the site. In this plan, heritage items listed by Schedule 5 Part 1 of the *Bathurst RLEP 2014* are coloured brown and numbered and Conservation Areas are hatched red and numbered. The subject site is coloured brown and numbered '139' and indicated by the red arrow.

² *Bathurst Regional Development Control Plan 2014*, Section 10.5.2 Statements of Significance, p. 181.

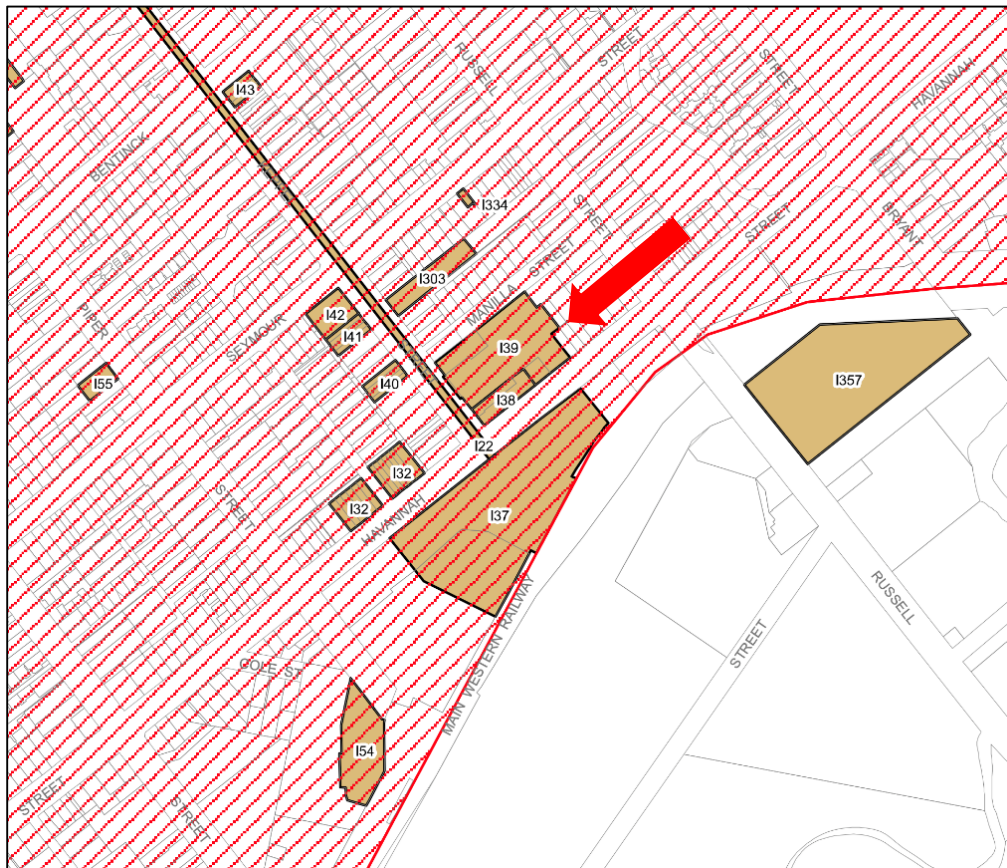



Figure 86: Detail, map showing heritage items and Conservation Areas in relation to the subject site.

Bathurst RLEP 2014

There are eight heritage items listed by Schedule 5 Part 1 of the *Bathurst RLEP 2014* within the vicinity of the site:



Item	Description	Significance
<p>'Victoria Hotel', No. 3 Keppel Street, Bathurst</p> <p><i>Bathurst RLEP 2014</i>, Item No. I38.</p>	<p>This item is located adjacent to the south of the site.</p> <p>It comprises a two-storey Victorian period hotel building.</p> <p>Significant view corridors towards the item are from either Keppel or Havannah Streets.</p> 	<p>The State Heritage Inventory provides the following Statement of Significance for this item:</p> <p><i>A major mid/late Victorian hotel serving as a reminder of the importance of the railway to Bathurst. Consisting of 2 parts, one single storied and one double, the building is an important streetscape element in vista down Keppel Street, framing railway station beyond. Built in circa 1880 to serve workers at the nearby 'Victoria Mill' (Later Tremaine's Mill) and so taking its name from it. Victorian Italianate in appearance it has excellent detail</i></p>

		<i>to parapet, string lines and arched window surrounds.³</i>
<p>'Webb's Commercial Building', No. 23 Keppel Street, Bathurst</p> <p><i>Bathurst RLEP 2014, Item No. I303.</i></p>	<p>This item is located to the north of the site at a distance of at least 75m.</p> <p>It comprises a two-storey Victorian period commercial building.</p> <p>Significant view corridors towards this item are from Keppel Street.</p> 	<p>The State Heritage Inventory provides the following Statement of Significance for this item:</p> <p><i>A substantial and elaborate late Victorian commercial building of high architectural quality. A prominent landmark, it contributes significantly to the streetscape. Designed by architect James Hine and connected to the Webb family interests.⁴</i></p>
<p>'Bathurst Street lamps', Keppel Street, Bathurst</p> <p><i>Bathurst RLEP 2014, Item No. I22.</i></p> <p>Listed on the State Heritage Register, ID No. 01666.</p>	<p>This item is located to the west of the site at a distance of at least 15m.</p> <p>It comprises a row of Victorian/Federation period streetlamps on the median strip.</p> <p>Significant view corridors towards the item are along Keppel Street.</p> 	<p>The State Heritage Inventory provides the following Statement of Significance for this item:</p> <p><i>The Bathurst Street and park lamp standards (105 in total) are a major element of nineteenth and early twentieth century street furniture that contributes to the distinctive historical character of the central civic and business areas of Bathurst. They are situated in Bathurst's central parks, Machattie and King's Parade and line the centre of six main streets. Installed between 1872 and 1924 when electricity replaced gas, the lamp posts are unusual in design, incorporating Victorian bases and Art Deco elements. The lamps posts are intact and in generally good condition. Comprising a large group of street lamps, they form a distinctive civic landmark in Bathurst and are rare in New South Wales.⁵</i></p>

³ 'Victoria Hotel', Heritage NSW State Heritage Inventory ID No. 1080164.



⁴ 'Webb's Commercial Building', Heritage NSW State Heritage Inventory ID No. 1080157.

⁵ 'Bathurst Street Lamps', Heritage NSW State Heritage Inventory ID No. 1080696.

<p>'Terraces', Nos. 20-26 Keppel Street, Bathurst</p> <p><i>Bathurst RLEP 2014, Item No. I40.</i></p>	<p>This item is located to the west of the site at a distance of at least 40m.</p> <p>It comprises a row of two-storey Victorian period terraces.</p> <p>Significant view corridors towards this item are from Keppel Street.</p> 	<p>The State Heritage Inventory provides the following Statement of Significance for this item:</p> <p><i>A largely intact row of late Victorian terrace houses. A type of building fairly rare in Bathurst. Fine cast iron detailing to balconies contributes significantly to the streetscape of Keppel Street. Built by J. Willett, who was also the builder for the Bathurst Hospital 1878.⁶</i></p>
<p>'Centennial Terraces', Nos. 36-42 Keppel Street, Bathurst</p> <p><i>Bathurst RLEP 2014, Item No. I41.</i></p>	<p>This item is located to the west of the site at a distance of at least 80m.</p> <p>It comprises a row of two-storey Victorian period terraces.</p> <p>Significant view corridors towards this item are from Keppel Street.</p> 	<p>The State Heritage Inventory provides the following Statement of Significance for this item:</p> <p><i>A representative row of terraced residences. A type of housing not common in Bathurst. Indicative of the increased need for housing created by the arrival of the railway in 1876. Being adjacent to Locksley House, they add significant variety to the streetscape.⁷</i></p>
<p>'Loxley House', No. 46 Keppel Street, Bathurst</p> <p><i>Bathurst RLEP 2014, Item No. I42.</i></p>	<p>This item is located to the west of the site at a distance of at least 110m.</p> <p>It comprises a Victorian period dwelling.</p> <p>Significant view corridors towards this</p>	<p>The State Heritage Inventory provides the following Statement of Significance for this item:</p> <p><i>A large residence of simple symmetrical Georgian lines in a prominent corner location which is a major townscape feature within the general conservation area. A rare example of a large Victorian Georgian townhouse in</i></p>

⁶ 'Terraces', Heritage NSW State Heritage Inventory ID No. 1080158.

⁷ 'Centennial Terrace', Heritage NSW State Heritage Inventory ID No. 1080156.

	<p>item are from Keppel or Seymour Streets.</p> 	<p><i>Bathurst (significance includes stables at rear).⁸</i></p>
<p>'Havannah Street group', Nos. 91-115 Havannah Street, Bathurst <i>Bathurst RLEP 2014, Item No. 132.</i></p>	<p>This item is located to the south of the site at a distance of at least 90m.</p> <p>It comprises a group of two-storey Victorian period terraces.</p> <p>Significant view corridors towards this item are from Havannah Street.</p> 	<p>The State Heritage Inventory provides the following Statement of Significance for this item:</p> <p><i>Together with the almost identical adjacent terrace (Street No. s 105 - 115), they form a dominant streetscape element. With their interesting brick detail and generally intact cast iron work, they form undoubtedly the most impressive streetscape of two storey terraces in Bathurst.⁹</i></p>
<p>'Bathurst Railway Station, Station Master's residence, cottage, Railway Institute, warehouse buildings and Ben Chifley Engine', Havannah Street, Bathurst <i>Bathurst RLEP 2014, Item No. 137.</i> Listed on the State Heritage</p>	<p>This item is located to the east of the site at a distance of at least 40m.</p> <p>It comprises a single-storey Victorian period railway station building.</p> <p>Significant view corridors are obtained from Havannah Street.</p> 	<p>The State Heritage Inventory provides the following Statement of Significance for this item:</p> <p><i>Bathurst station group is one of the major country railway sites in NSW with a substantial first class station, residence and workshop group. The station is located at the end of Keppel St, one of the main streets of Bathurst and the buildings form a significant civic group in the town of Bathurst, particularly with the location of the residences and engineer's office in Havannah St and the orientation of the station building to the town. The design of Bathurst station building is</i></p>

⁸ 'Loxley House', Heritage NSW State Heritage Inventory ID No. 1080155.

⁹ 'Havannah Street Group', Heritage NSW State Heritage Inventory ID No. 1080112.

Register, ID No. 01078.		<i>different to any other building on the system and reflects a one-off approach not often seen, but respecting the importance of the largest city west of the mountains.¹⁰</i>
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4.3 Integrity

The site has undergone significant change since the establishment of Tremain’s Mill in the late 19th century, as well as since its closure in the early 1980s. Parts of the site have been adaptively re-used for commercial and residential purposes, while other buildings have been removed. Refer to the *CMP March 2016* for further information.

4.4 Grading of Significance

For the following, refer to Figure 87, which reproduces the revised Grading of Significance for the site as prepared by Weir Phillips Heritage and Planning.



Figure 87: Revised Grading of Significance. The current site boundaries are outlined in black. Karl Walls Design, 2022; annotations by WPH&P.





The following table defines the criteria used for the Grading of Significance as set out by the *CMP 2016*:







¹⁰ 'Bathurst Railway Station, Station Masters Residence, Cottage, Railway Institute and Warehouse Buildings', Heritage NSW State Heritage Inventory ID No. 1080695.

Grading	Justification
Exceptional	Where an individual space or element is assessed to display a strong degree of correlation with the Statement of Significance for the place. Spaces, elements or fabric exhibit a high degree of intactness and quality, though minor alterations or degradation may be evident.
High	Where an individual space or element is assessed to display a substantial degree of correlation with the Statement of Significance. Spaces, elements or fabric, originally of substantial quality, and which may have undergone considerable alteration or adaptation resulting in presentation which is either incomplete or ambiguous. This category also includes spaces, elements or fabric of average quality in terms of design and materials, but which exhibit a high degree of intactness.
Moderate	Where an individual space or element is assessed to display a moderate degree of correlation with the Statement of Significance. Spaces, elements or fabric, originally of little intrinsic quality and which may have undergone alteration or degradation. Or, in addition, are of a relatively new construction and the assessment of significance is difficult or is poor quality in relation to elements of exceptional/high significance. This category also includes spaces, elements or fabric of any quality which have undergone extensive alteration or adaptation.
Low	Where an individual space or element is assessed to display a low degree of correlation with the Statement of Significance, particularly compared with other features of the item. Spaces, elements or fabric, originally of little intrinsic quality and which may have undergone alteration or degradation. This category also includes spaces, elements or fabric of any quality but which have undergone extensive alteration or adaptation to such an extent that only isolated remnants survive, resulting in a low degree of intactness and quality of presentation.

Neutral	Where an individual space or element is assessed to display a minor degree of correlation with the Statement of Significance. Spaces, elements or fabric that have been assessed as possessing little or no significance.
Intrusive	Where an individual space or element detracts from the appreciation of cultural significance, by adversely affecting or obscuring other significant areas, elements or items.

The following elements are located within the boundaries of the area of the proposed works.

Element	Significance	Photograph
Invincible Flour Sign (A)	Exceptional Significance	
Victoria Stores Building (B)	High Significance	
Mill Building (C)	Exceptional Significance. This grading relates to the principal brick form of the building. Accretions such as the corrugated structure on the roof are of Low Significance.	
Bedwells Feed Barn Warehouse (D) North and South Additions	Exceptional Significance Low Significance	

Building 11 (E)	Exceptional	
Timber Silos (F)	Exceptional	
Concrete Silos (G)	High	
Doppio's Hair Container (H)	Intrusive	
Fusion Hair Container (I)	Intrusive	
Amenities Block (J)	Intrusive	

Mill Office (K)	Low Significance	
Mill Building Staircase	Low Significance	
Manilla Street Shed (L)	Low Significance	
Weighbridge and Siding (including industrial artefacts) (M)	High Significance	
Weighbridge Shed	Low Significance	
Bedwells Rear Warehouse (N)	Low	
Building 16 (O)	Low/Moderate	
Garages (P)	Intrusive	

4.5 View Corridors

The principal view corridor towards the site is from directly outside on either Keppel or Havannah Street. The site is prominent with the streetscape, particularly the Mill Building, Timber Silos and Concrete Silos, which are of a greater scale than other structures. These

structures retain a high degree of visibility on approach from the north on Keppel Street, the east and west on Havannah Street and from the east on Russell Street and Manilla Street. The site is also visible from the Bathurst Railway Station.

Refer to Figures 88 to 93 which illustrate the primary view corridors towards the site.



Figure 88: Looking directly through the site from the west side of Keppel Street.



Figure 89: Looking towards the site from the north on Keppel Street.



Figure 90: Looking towards the site from the south next to Bathurst Railway Station.



Figure 91: Looking towards the site from the north on Keppel Street.



Figure 92: Looking towards the site from the east on Havannah Street.



Figure 93: Looking towards the site from the east on Russell Street.

5 THE PROPOSAL

The Bathurst Mill Development company has prepared a Master Plan to guide future planning and design for development of the former Tremain's Flour Mill located at 7 Keppel Street and 67 Havannah Street, Bathurst.

The proposed development will transform the former Tremain's Flour Mill and surrounding precinct into a thriving mixed use adaptive reuse project. This will include commercial and educational spaces, food, beverage and retail with publicly accessible spaces and some residential dwellings.

The adaptive re-use of the precinct includes upgrades, alterations and additions to existing buildings of heritage significance – The Mill Building, Bedwells Barn, Timber Silos and Concrete Silos with removal of some existing structures. There will also be various new complementary buildings within the existing Tremain's Flour Mill precinct.

The Concept Development Application and proposed Master Plan provides a range of opportunities for site development including:

Hotel Accommodation

Various accommodation across the site located in a combination of new and adaptive re-use buildings comprising:

- 9 room heritage boutique hotel in existing 1880s Victoria Stores Building. Development Application approved.
- 51 room new boutique hotel fronting Keppel St with rooms ranging from 32sqm to 46 sqm.
- 52 room new boutique hotel within the Havannah St precinct with rooms ranging from 33 sqm to 45 sqm.
- 10 room boutique hotel in existing concrete silos with 20sqm sized rooms.

Commercial

Range of commercial offerings across the site comprising:

- Hotel reception, guest library and lounge.
- Arts exhibition space.
- Farmers Market.
- Bedwells Barn catering for a regular farmers market and multi-functional wedding and event space.
- Commercial kitchen.
- 240 sqm roof top bar located at the roof level at the existing concrete silos.
- 90 seat art-house cinema and event space.
- Office, gallery, exhibition, studio space over four floors of commercial space plus associated 6 x 1 bedroom artist in residence studios.

Retail

Wide range of complementary mixed-use retail, food and beverage offerings comprising 28 tenancies:

- Cafes
- Enoteca
- Restaurants
- Distillery
- Assorted bespoke retail – bookshop, small goods, beauty salon, haberdashery, hardware, homewares, flower shop, hair salon, wellness centre.
- Assorted bespoke food and beverage – artisan bakery, fromagerie and delicatessen, butchery.

Residential

Various residential offerings at the rear of the site comprising:

- 12 x 3 bedroom plus study terraces facing Manilla Street

- 4 x 2 bedroom plus study mews

Carparking

- 46 x on site car spaces
- 21 x angled car spaces on Havannah St frontage.

Refer to Figures 94 to 96 which show the proposed plan for the retention and removal of existing buildings; the proposed site plan including the footprints of new buildings; and the proposed massing.

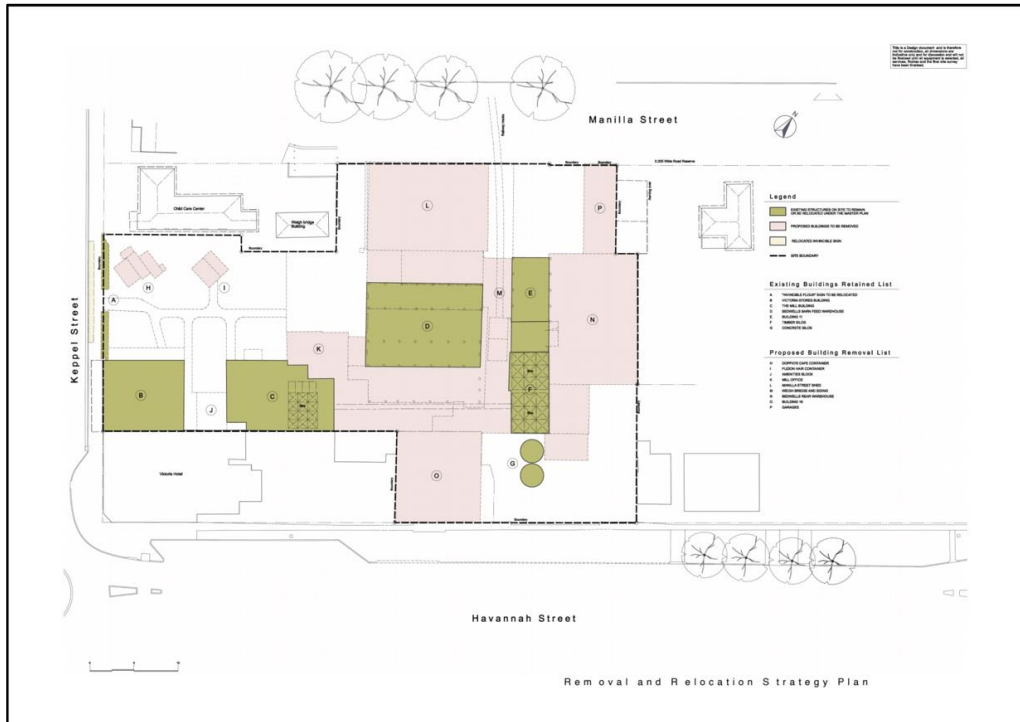


Figure 94: Existing plan showing retention and removal of existing built elements.
Karl Walls Design, 2022

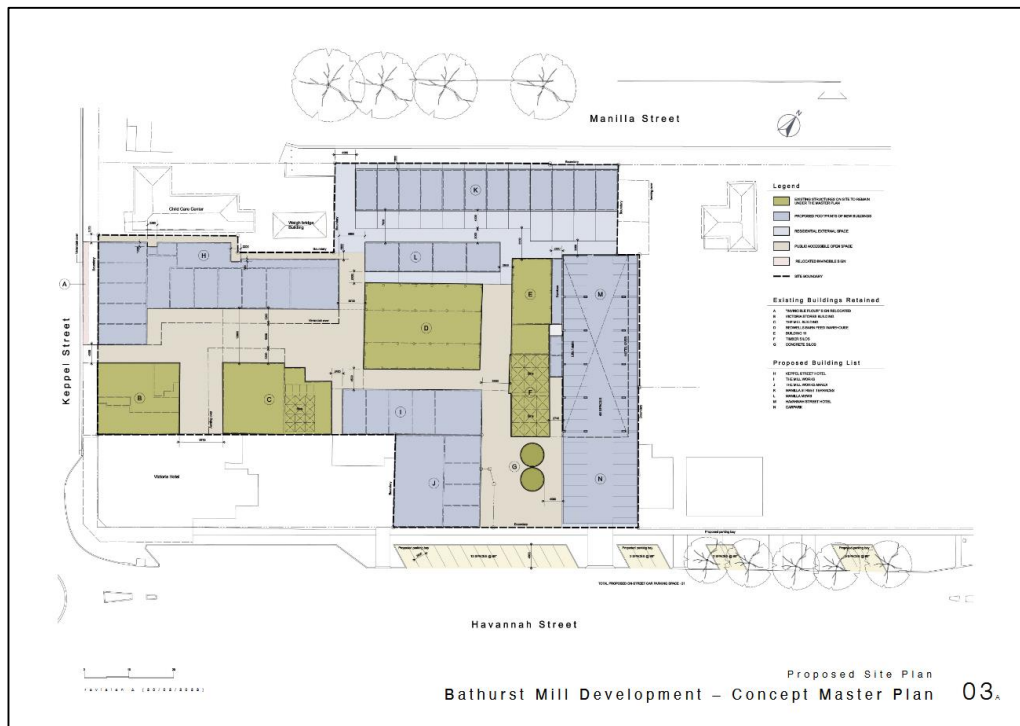


Figure 95: Proposed site plan showing retained and new built elements.
Karl Walls Design, 2022

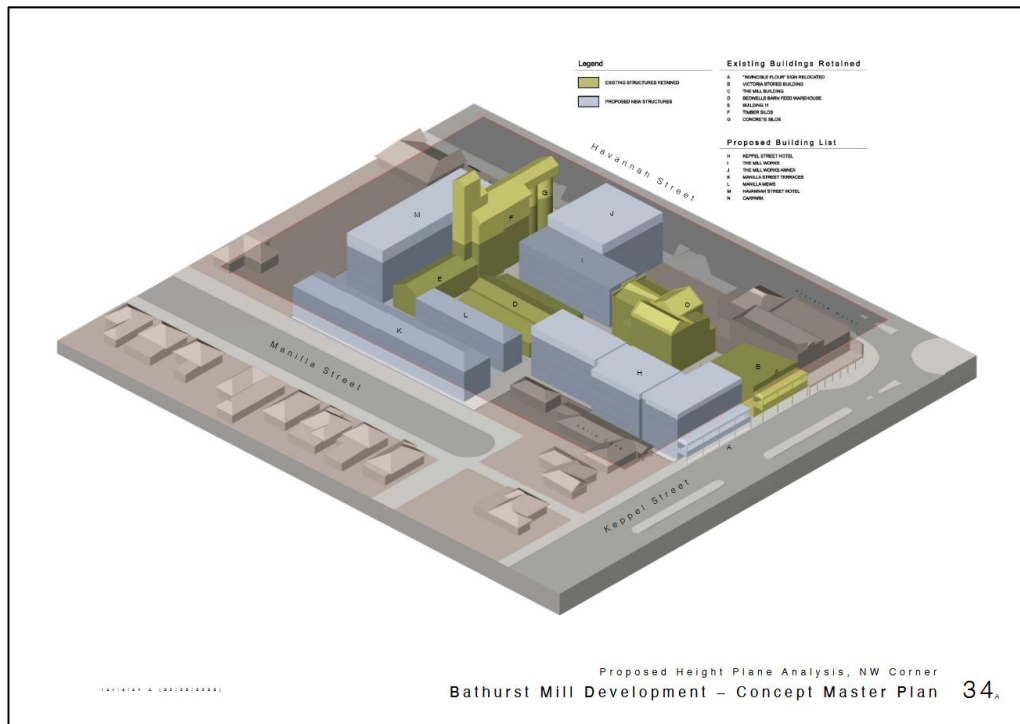


Figure 96: Height Plane Analysis showing proposed massing of buildings.
Karl Walls Design, 2022

6 EFFECT OF WORK

6.1 Methods of Assessment

The following is a merit-based assessment. It does not consider compliance or otherwise with Council's numerical controls except where non-compliance would result in a heritage impact. Refer to the Statement of Environmental Effects that accompanies this application. The following assessment is made with an understanding of the objectives and controls provided by the *Bathurst RLEP 2014* and the *Bathurst RDCP 2014*.

6.2 Effect of Work on Former Tremain's Flour Mill

The proposed works will have an acceptable impact on the site for the following reasons:

- The proposed redevelopment of the site will allow the raising of capital which is necessary to facilitate its conservation and ongoing maintenance. It is unlikely that the site will ever be used as a mill again. Some of the existing buildings have already been successfully adaptively re-used for other purposes since the closure of the mill, while others have been left in varying states of degradation.
- All buildings which have been graded as Exceptional or High Significance will be retained and adaptively re-used for a range of new uses. The buildings all demonstrate a strong relationship to the historical development of the site; are aesthetically significant and prominent within key view corridors; and contribute towards an understanding of the site as a former mill precinct. These buildings can be easily converted for the range of uses proposed with significant fabric conserved.
- The 'Invincible Flour' sign will be relocated to the Keppel Street footpath, close to its current location, to ensure its conservation and facilitate the construction of a new hotel building. The sign can be carefully removed and installed on the footpath. It will continue to remain a highly significant element that bears a clear historic and visual relationship to the former mill precinct.
- The proposed removal of built elements has been confined to those which are graded Intrusive, Neutral, Low and Moderate, which is regarded as acceptable by the *CMP March 2016*. These buildings are typically later elements such as the Amenities Block; are not aesthetically and historically significant or intrinsic towards understanding the site; and visually detract from more significant elements.
- Their removal will have a minimal and acceptable impact and will enable a better understanding of the built form of retained elements. This is demonstrated by the removal of the external staircase, which will help to partly reinstate its original appearance. Where required, moveable elements such as remnant machinery will be relocated and conserved.
- Other accretions such as the Mill Office and the corrugated roof structure to the Mill Building, while both clearly of some age, are not integral towards understanding the use of the site and are visually intrusive in contrast to the more significant brick building form of the Mill Building. They would also likely require substantial remedial work if retained, given their condition.
- The proposed new buildings will sit comfortably in the site as they will be of an appropriate massing and scale and will maintain predominant setbacks and rooflines in relation to the site. They will be well-designed with a simple aesthetic that does not detract from the more aesthetically significant retained buildings or compete with their architectural detailing.

- The proposed new building envelopes will be further refined through future detailed Development Applications, and this will reduce the bulk and scale as articulation is introduced.
- The proposed car parking will have an acceptable impact because it is located outside of any significant view corridors and will present, where visible, as visually recessive to retained significant elements.
- While it is accepted that a basement car park would have less of a visual impact and provide more potential car spaces, it is highly likely that any excavation would result in the destabilisation, and ultimately collapse, of significant elements above. The minor visual impact of the proposed surface car parking is in this instance preferable to risking any physical impact to significant fabric.
- The proposed works will reduce some view corridors towards the site when standing directly outside on Keppel Street. This has been mitigated by retaining a through site link so that view of retained buildings of smaller scale, such as the Bedwell Feed Barn Warehouse, can continue to be seen and appreciated from the public domain.
- View corridors towards the site from both Havannah and Manilla Streets will also, to some extent, be reduced. This will have a minimal and acceptable impact because these views have historically been blocked from view for some time due to the existing surrounding structures. Again, the proposal has reduced the impact by introducing through site links from the public domain.
- The majority of retained buildings will continue to remain visually prominent in significant view corridors towards the site in the public domain, including the Timber Silos and Concrete Silos and the Mill Building. This has been achieved by sensitively siting the new buildings which will be of substantially smaller scale than existing.
- The proposed works will, overall, have an acceptable impact because the site will continue to be read and understood as a former mill, as demonstrated by the retention of buildings which clearly demonstrate this historic relationship, while the removal of detracting elements will enable a better understanding of their built form. The proposed works will help to facilitate the ongoing conservation of the site for the long-term.

6.3 Effect of Work on Bathurst Heritage Conservation Area

The proposed works will have an acceptable impact on the Bathurst Heritage Conservation Area for the following reasons:

- The proposed removal of buildings with little or no significance will allow a better understanding of more significant retained elements. They comprise buildings which are of little or no aesthetic and historic value and make relatively minor contributions towards understanding the development of the site and Conservation Area. Their removal will have a minimal and acceptable impact.
- The proposed retention and adaptive reuse of buildings will have a positive impact by facilitating the ongoing conservation of significant elements and the wider precinct. These elements include the silos and Mill Building which are intrinsic towards understanding the former use of the site as a mill and its historic relationship to the Conservation Area. They can be easily configured for the proposed uses while enabling the retention of their original fabric.
- The proposed car parking will have an acceptable impact because it is located outside of any significant view corridors and will present, where visible, as visually

recessive to retained significant elements.

- While it is accepted that a basement car park would have less of a visual impact and provide more potential car spaces, it is highly likely that any excavation would result in the destabilisation, and ultimately collapse, of significant elements above. Given these are key landmark items, this would have a significant negative impact on the broader Conservation Area. The minor visual impact of the proposed surface car parking is in this instance preferable to risking any physical impact to significant fabric.
- The proposed new buildings will sit comfortably in the Conservation Area as they will be of an appropriate massing and scale and will maintain predominant setbacks and rooflines in the Conservation Area. They will be well-designed with a simple aesthetic that does not detract from the more aesthetically significant retained buildings or compete with their architectural detailing. As noted above, future Development Applications will further refine the proposed building envelopes and reduce the bulk and scale with articulation.
- The proposed works will reduce some view corridors towards the site when standing directly outside on Keppel Street. This has been mitigated by retaining a through site link so that view of retained buildings of smaller scale, such as the Bedwell Feed Barn Warehouse, can continue to be seen and appreciated from the Conservation Area.
- The majority of retained buildings will continue to remain visually prominent in significant view corridors towards the site in the Conservation Area, including the Timber Silos and Concrete Silos and the Mill Building. This has been achieved by sensitively siting the new buildings which will be of substantially smaller scale than existing.
- The proposed works will, overall, have an acceptable impact on the Conservation Area because the site will continue to read as a former mill through the retention of elements which clearly demonstrate this historic use, while the removal of detracting elements will enable a better understanding of their built form. The proposed works will help to facilitate the ongoing conservation of the site for the long-term.

6.4 Effect of Work on Heritage Items Within the Vicinity

‘Victoria Hotel’, No. 3 Keppel Street, Bathurst

The proposed works will have a minimal and acceptable impact on this item for the following reasons:

- There will be no impact on the fabric of this item, which is sufficiently separated from any works requiring removal or construction of buildings.
- There will be no impact on significant view corridors towards this item, which are obtained from directly outside on either Keppel or Havannah Street. The site is located to the north and east outside of these view corridors.
- The proposed works involving the removal, retention and construction of buildings, will be visible from this item. This will have a minimal and acceptable impact for the following reasons:
 - Buildings proposed for removal that are visible from the item include contemporary shipping containers, 20th century period sheds and other detracting elements. Their removal will enhance the setting of the item.

- Buildings proposed for retention that are visible from the item include the Mill Building, Timber Silos and Concrete Silos and Bedwells Feed Barn Warehouse. Significant view corridors from the item towards the site will be retained.
- Buildings proposed for construction will be of an appropriate massing and scale that relates to their built context. They will not present as overly prominent or detract visually from retained built elements.
- The proposed hotel building will maintain the predominant front setback to the street so that view corridors towards the item from the north on Keppel Street are retained.
- The proposed works will, overall, have no impact on the ability of the public to understand and appreciate the historic and aesthetic significance of the item because neither its fabric nor significant view corridors will be impacted on, while its setting will be enhanced by the removal of detracting elements.

‘Bathurst Railway Station, Station Master’s residence, cottage, Railway Institute, warehouse buildings and Ben Chifley Engine’, Havannah Street, Bathurst

The proposed works will have a minimal and acceptable impact on this item for the following reasons:

- There will be no impact on the fabric of this item, which is sufficiently separated from the site by at least 40m.
- There will be no impact on significant view corridors towards this item, which are obtained from directly outside on Havannah Street. The site is located to the north outside of these view corridors.
- The proposed works involving the removal, retention and construction of buildings, will be visible from this item. This will have a minimal and acceptable impact for the following reasons:
 - Buildings proposed for removal that are visible from the item include Building 16. It is noted that this building is largely concealed from view by vegetation and intervening structures and is not an important element in its wider setting.
 - Buildings proposed for retention that are visible from the item include the Concrete Silos and Timber Silos. Significant view corridors from the item towards the site will be largely retained.
 - Buildings proposed for construction will be of an appropriate massing and scale that relates to their built context. They will not present as overly prominent or detract visually from retained built elements.
- The proposed works will, overall, have no impact on the ability of the public to understand and appreciate the historic and aesthetic significance of the item because neither its fabric nor significant view corridors will be impacted on, while its setting will be enhanced by the removal of detracting elements.

‘Bathurst street lamps’, Keppel Street, Bathurst

The proposed works will have a minimal and acceptable impact on this item for the following reasons:

- There will be no impact on the fabric of this item, which is sufficiently separated from the site by at least 15m.
- There will be no impact on significant view corridors towards this item, which is obtained from directly outside on Keppel Street. The site is located to the east outside of these view corridors.
- The proposed works involving the removal, retention and construction of buildings, will be visible from this item. This will have a minimal and acceptable impact for the following reasons:
 - Buildings proposed for removal that are visible from the item include contemporary shipping containers and 20th century period sheds that are visually intrusive. Their removal will enhance the setting.
 - Buildings proposed for retention that are visible from the item include the silos, Mill Building, Victoria Stores Building and Barnwells Feed Barn Warehouse. Significant view corridors from the item towards the site will be largely retained.
 - Buildings proposed for construction will be of an appropriate massing and scale that relates to their built context. They will not present as overly prominent or detract visually from retained built elements.
 - It is noted, however, that the proposed hotel building will block view of the site from Keppel Street. This has been mitigated by maintaining a through site link which will help to preserve view corridors from the public domain towards retained buildings of smaller scale set into the site.
- The proposed works will, overall, have no impact on the ability of the public to understand and appreciate the historic and aesthetic significance of the item because neither its fabric nor significant view corridors will be impacted on, while its setting will be enhanced by the removal of detracting elements.

**‘Webb’s Commercial Building’, No. 23 Keppel Street, Bathurst;
‘Terraces’, Nos. 20-26 Keppel Street, Bathurst;
‘Centennial terraces’, Nos. 36-42 Keppel Street, Bathurst;
‘Loxley House’, No. 46 Keppel Street, Bathurst**

The proposed works will have a minimal and acceptable impact on these items for the following reasons:

- There will be no impact on the fabric of these items, which are sufficiently separated from the site by between 40m and 110m.
- There will be no impact on significant view corridors towards these items, which are obtained from directly outside on Keppel Street to the north. The site is located to the south outside of these view corridors.
- The proposed works involving the removal, retention and construction of buildings, will be visible from this item. This will have a minimal and acceptable impact for the following reasons:
 - Buildings proposed for removal have essentially no visibility from the item and do not form part of their setting.
 - Buildings proposed for retention that are visible from the items include the silos, Mill Building and Victoria Stores Building, all of which form part of

significant view corridors from the items.

- Buildings proposed for construction will be of an appropriate massing and scale that relates to their built context. They will not present as overly prominent or detract visually from retained built elements.
- The proposed works will, overall, have no impact on the ability of the public to understand and appreciate the historic and aesthetic significance of item because neither its fabric nor significant view corridors will be impacted on, while its setting will be enhanced by the removal of detracting elements.

‘Havannah Street group’, Nos. 91-115 Havannah Street, Bathurst

The proposed works will have a minimal and acceptable impact on this item for the following reasons:

- There will be no impact on the fabric of this item, which is sufficiently separated from the site by a distance of at least 90m.
- There will be no impact on significant view corridors towards this item, which are obtained from directly outside on Havannah Street. The site is located well to the east and set back from Havannah Street.
- The proposed works involving the removal, retention and construction of buildings, will be visible from this item. This will have a minimal and acceptable impact for the following reasons:
 - Buildings proposed for removal have essentially no visibility from the item and do not form part of its setting.
 - Buildings proposed for retention that are visible from the item include the silos and Mill Building, both of which form part of its skyline view of the surrounding area.
 - Buildings proposed for construction will be of an appropriate massing and scale that relates to their built context. They will not present as overly prominent or detract visually from retained built elements.
- The proposed works will, overall, have no impact on the ability of the public to understand and appreciate the historic and aesthetic significance of the item because neither its fabric nor significant view corridors will be impacted on, while its setting will be enhanced by the removal of detracting elements.

7 CONCLUSION

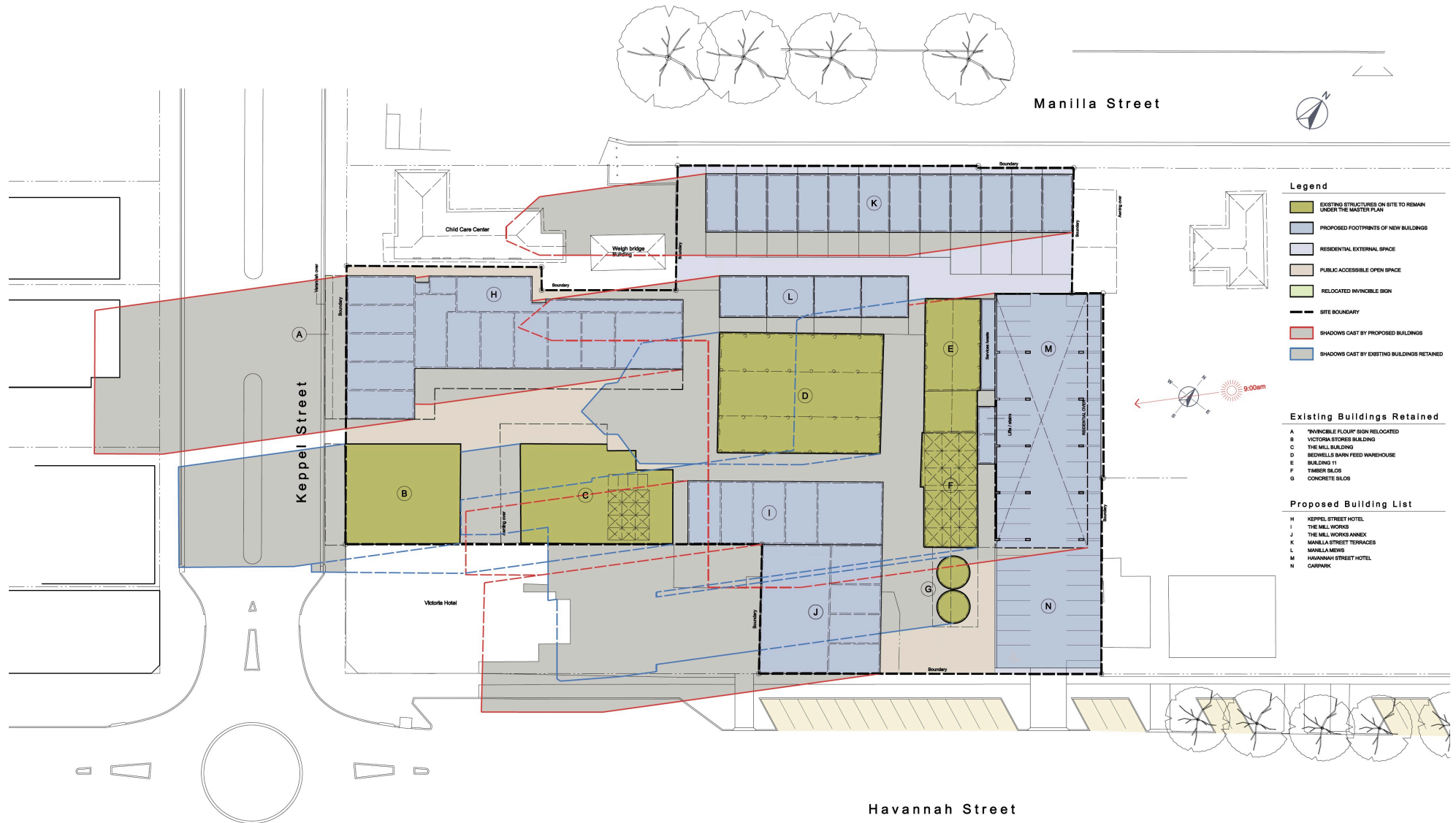
This Heritage Impact Statement has been prepared in conjunction with a Concept Development Application for the redevelopment of the former Tremain’s Flour Mill at No. 7 Keppel and No. 67 Havannah Street, Bathurst, New South Wales. The site is located within the Bathurst Regional Council area. The principal planning control for the site is the *Bathurst Regional Local Environmental Plan 2014 (RLEP 2014)*. The site is listed as a heritage item (“Tremain’s Mill & Silos (formerly Victoria Mill) & Invincible Flour Sign”) and is located within the Bathurst Heritage Conservation Area as defined by Schedule 5 Parts 1 and 2 of the *RLEP 2014*. The site is located within the vicinity of heritage items listed by this Schedule and items listed on the State Heritage Register under the *NSW Heritage Act 1977*.

The proposed works will have an acceptable impact on the former flour mill because significant built elements will be retained and detracting elements of little significance removed which will allow the public to better read and appreciate the site as a former mill precinct. The adaptive reuse of retained buildings can be achieved with minimal impact on significant fabric and has been undertaken at the site since it ceased to be used as a mill in

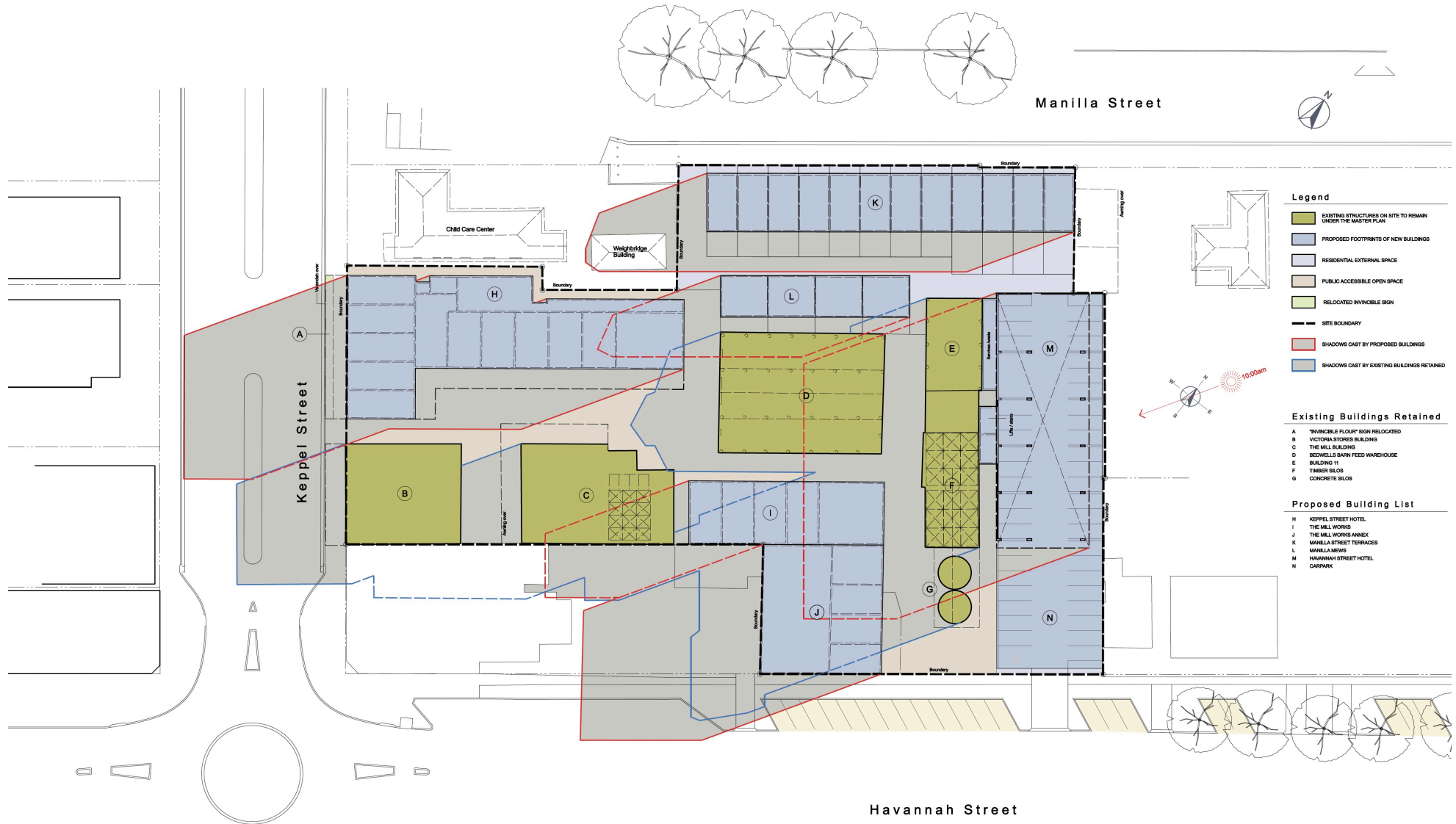
the 1980s. The new works are of an appropriate massing and scale and have been sensitively sited to minimise their impact on significant view corridors and the built context. The proposed works will allow the raising of capital which will enable the ongoing conservation of the site.

The proposed works will have a minimal and acceptable impact on heritage items in the vicinity because neither their fabric nor significant view corridors towards them will be impacted. Where visible, removed elements that are detracting will enhance their wider setting, while retained buildings will ensure there is minimal impact on view corridors from the items. The new works will be of an appropriate massing and scale and will not detract visually or present as overly prominent when viewed from the items.

The proposed works fulfil the aims and objectives of the *RLEP 2014* and the *DCP 2014* by improving the quality and diversity of hotel, retail, commercial and residential options in Bathurst while respecting the heritage significance of the area in which it lies.

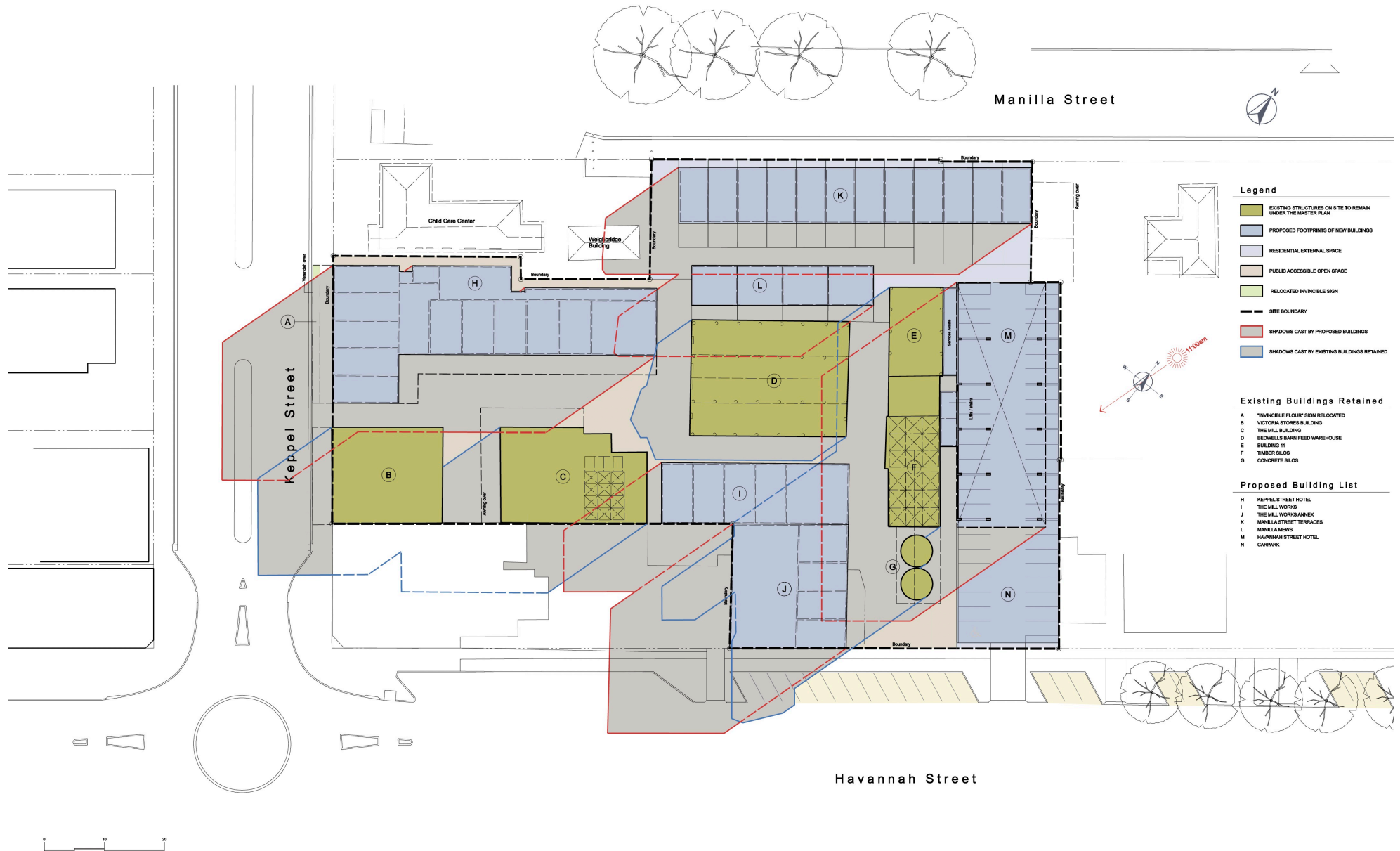


Shadows - 9:00am 21st June

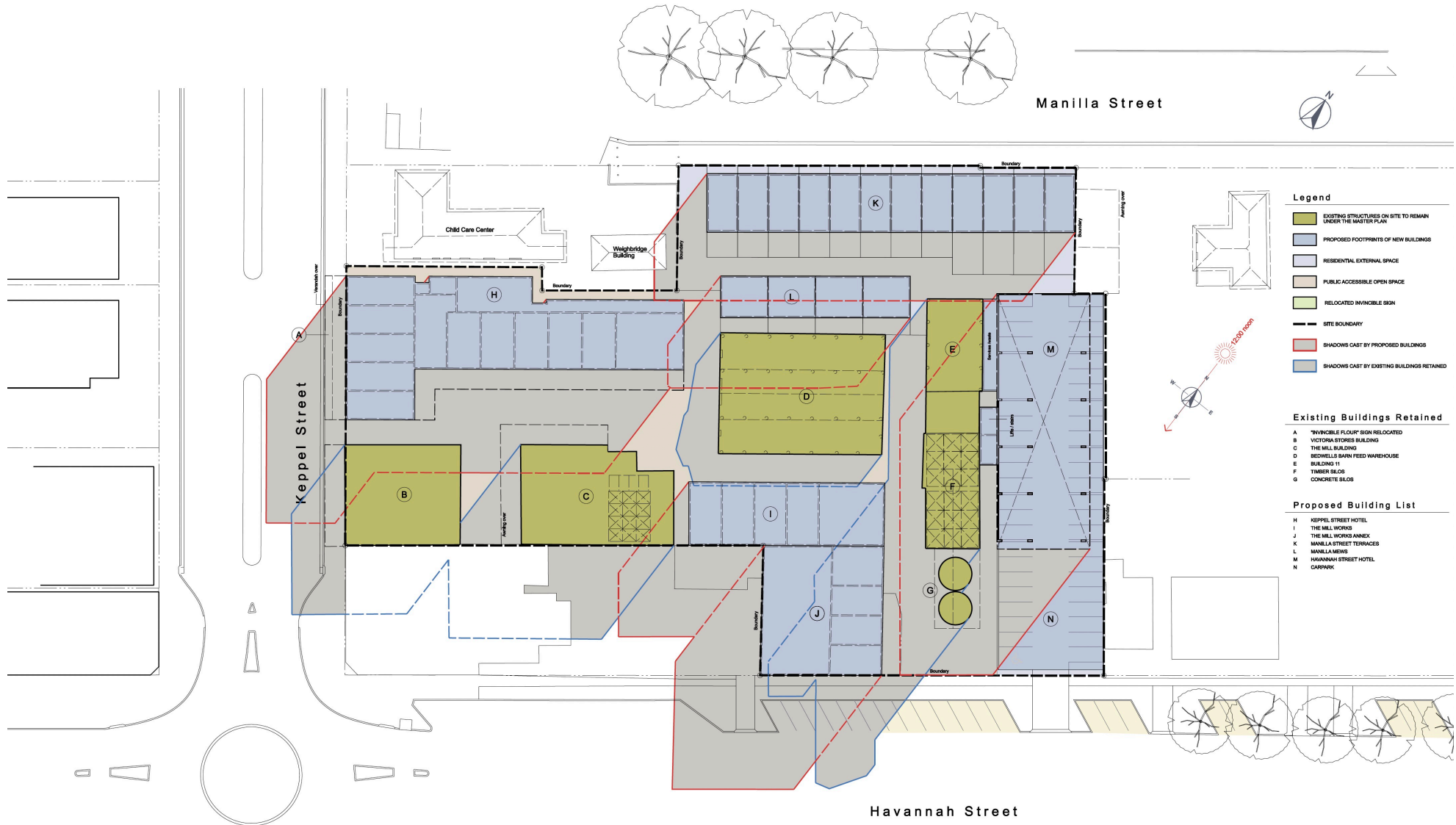


Shadows - 10:00am 21st June

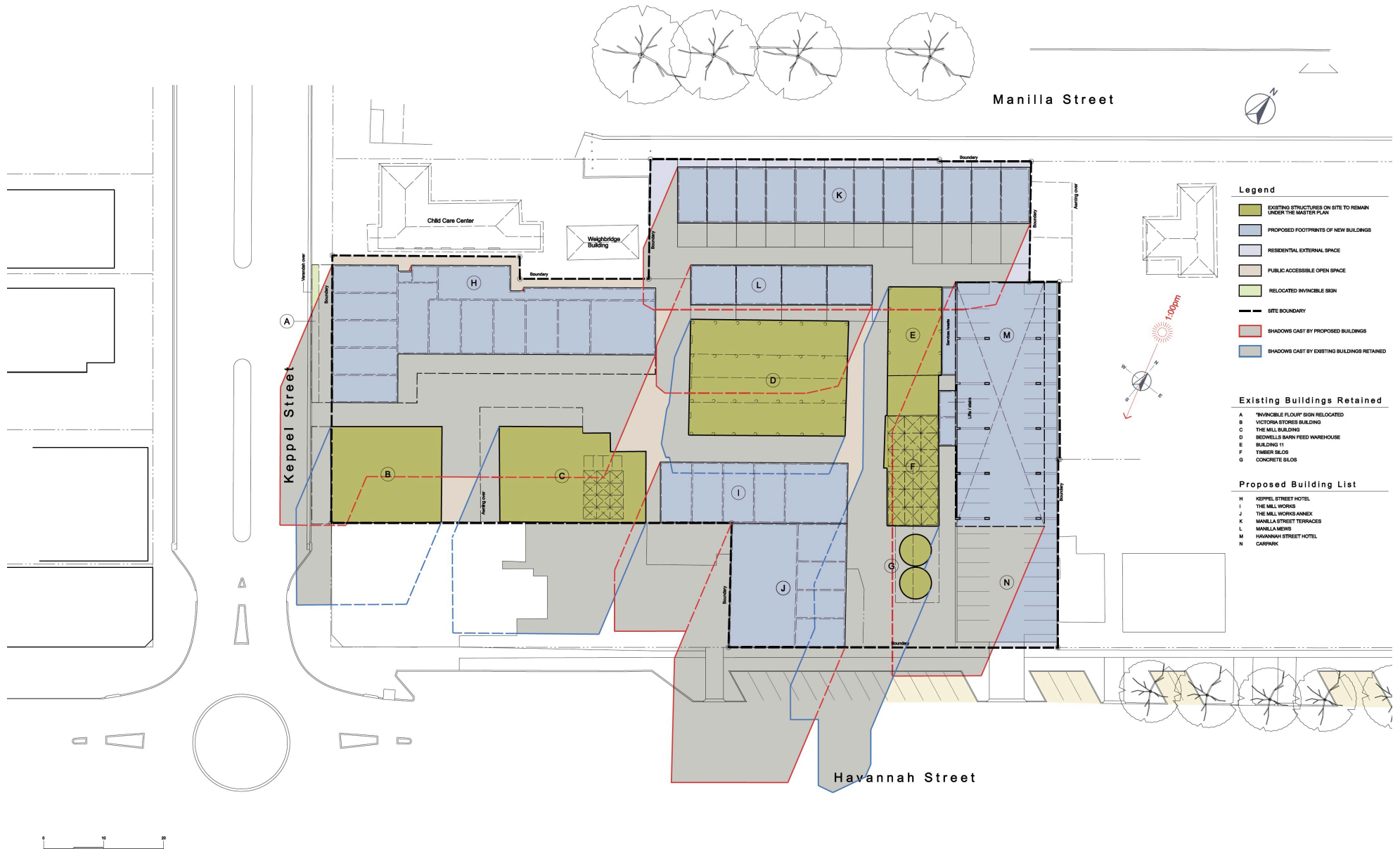
Bathurst Mill Development – Concept Master Plan 25_A



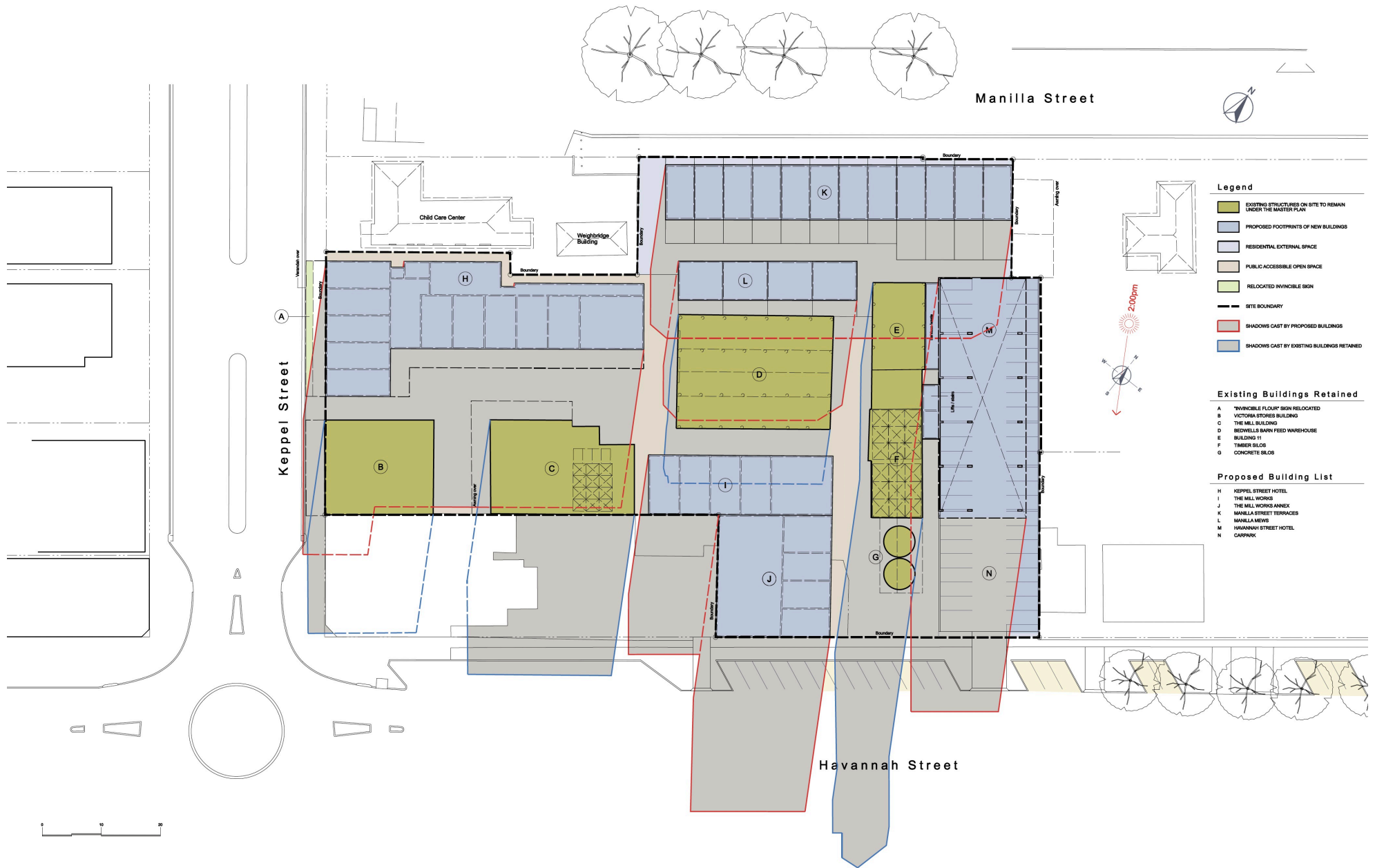
Shadows - 11:00am 21st June



Shadows - 12:00noon 21st June



Shadows - 1:00pm 21st June



- Legend**
- EXISTING STRUCTURES ON SITE TO REMAIN UNDER THE MASTER PLAN
 - PROPOSED FOOTPRINTS OF NEW BUILDINGS
 - RESIDENTIAL EXTERNAL SPACE
 - PUBLIC ACCESSIBLE OPEN SPACE
 - RELOCATED INVINCIBLE SIGN
 - SITE BOUNDARY
 - SHADOWS CAST BY PROPOSED BUILDINGS
 - SHADOWS CAST BY EXISTING BUILDINGS RETAINED

- Existing Buildings Retained**
- A "INVINCIBLE FLOOR" SIGN RELOCATED
 - B VICTORIA STORES BUILDING
 - C THE MILL BUILDING
 - D BEEWELLS BARN FEED WAREHOUSE
 - E BUILDING 11
 - F TIMBER BLOCS
 - G CONCRETE BLOCS

- Proposed Building List**
- H KEPPEL STREET HOTEL
 - I THE MILL WORKS
 - J THE MILL WORKS ANNEX
 - K MANILLA STREET TERRACES
 - L MANILLA MEWS
 - M HAVANNAH STREET HOTEL
 - N CARPARK

Shadows - 2:00pm 21st June



revision A (20/02/2023)

Emma Castle

From: Michele & David Quah <quahlity1@bigpond.com>
Sent: Wednesday, 14 December 2022 7:20 AM
To: Council
Subject: Tremain Mill Development. (DA no. 2022/523)

CAUTION: This email originated from outside BRC. Do not click links or open attachments unless you recognise the sender and know the content is safe.

Hi Emma,

As discussed, we are pleased with the proposed beautification development of the Tremain Mill complex in DA application No. 20022/523. However, the traffic and parking at this end of Keppel St is just so horrendous even now. With Hotel accommodation rooms and the various Retail outlets there is just not enough parking space allocation on site for this development for Residence/Retail Workers let alone customers. No doubt each Retail outlet may have more than one worker, requiring more than a single car park allocation for each. Currently, residences are finding it difficult to park and virtually impossible for their friends and family to call in especially during the day time. It is also very difficult to drop or pick up grandchildren as all the parking spaces down this end of Keppel St is full.

We are always anxious coming out of our garage onto Manilla Lane as quite often cars would just race down this Lane. We need to have a Speed Hump at the junction between Manilla St/ Manilla Lane and one in the middle of Manilla Lane to slow down the traffic. With the proposed increased activities, It is just a matter of time before there will be a nasty accident in such close proximity to the Child Care Centre.

Having such a major development in such a congested area, both the parking availability and traffic safety issues down Manilla Lane will need to be addressed first. Thanking you in anticipation.

Yours sincerely,
Michele & David Quah
17 Keppel St,
Bathurst, NSW2795
0457970666.
Sent from my iPad

Emma Castle

From: Warren Stocks <Warren.Stocks@scotsallsaints.nsw.edu.au>
Sent: Thursday, 15 December 2022 3:17 PM
To: Council
Subject: Redevelopment of 7 Keppel St, Bathurst - Tremains Mill and Silo

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Hi,

I wish to raise my concerns regarding the proposed redevelopment of 7 Keppel St, Bathurst - Tremains Mill and Silo. The general concept looks fantastic but the absent of any parking for an 80 room motel is just simply foolishness. I really am not sure where it is expected these guests are to park – maybe Carrington Oval? There is also no parking options for the any of the retail/hospitality stores nor their customers. As a local of Lord Street, I am keenly aware of the limited parking options that are currently available at times in the immediate area, this would be significantly amplified if this redevelopment was to approved in its current form.

I also have concerns regarding the height of the motel facing onto Keppel and do not believe this is in keeping this the heritage principles which Bathurst has attempted to maintain over the last few decades. I can be contacted on 0438 314625 for further discussion.

Kind Regards,

Warren Stocks

Emma Castle

From: Natalia Bradshaw <nataliaottolenghi1@gmail.com>
Sent: Tuesday, 27 December 2022 10:15 PM
To: Council
Subject: Redevelopment of Tremain Mill - 10-2022-523-1

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Dear Mr Sherley,

I've been made aware of the proposed redevelopment of the Tremain Mill in Bathurst and want to write to applaud Bathurst Regional Council for considering this redevelopment.

I had to write to you because it is not often that such an opportunity is created; as someone prominent in the creative industries, it would be very exciting to see this redevelopment happen.

If only more regional councils considered such an excellent development that is not only financially sound, but will add so much to the community with its exceptional design principles across residential, commercial and communal spaces.

As someone who is passionate about creative ecosystems, I hope that this development gets approved so that we can have such a marvellous destination in NSW. Bathurst Mill will be a terrific destination for those wanting to move out of cities into beautiful regional areas like Bathurst, for those wanting quality road trip destinations, and for existing residents who love and are proud of their area.

The exceptional design principles proposed will attract the best in food, arts and related industries.

I do hope this becomes a reality!

Yours sincerely,

Natalia Bradshaw

NATALIA OTTOLENGHI BRADSHAW

chair unsw galleries/governor australian museum/art advocate/curator

10 wylde st potts point nsw 2011

M: +61411628727

Instagram: nataliaottolenghib

W: www.nataliabradschaw.com

LinkedIn: <https://www.linkedin.com/in/natalia-bradshaw-nee-ottolenghi-42890b29/>

Emma Castle

From: Vickie Seymour <vseymour10@gmail.com>
Sent: Sunday, 15 January 2023 2:01 PM
To: Council
Subject: Attn: General Manager regarding Concept DA (10 - 2022 - 523 - 1)

CAUTION: This email originated from outside BRC. Do not click links or open attachments unless you recognise the sender and know the content is safe.

To the General Manager, Bathurst Regional Council

Regarding Concept DA (10 - 2022 - 523 - 1)

Concept Development Application - Redevelopment of Tremain Mill

My concerns are as follows –

- Lack of consultation with the Bathurst Community, including the fact this DA concept was advertised over the holiday period. This doesn't seem appropriate for such a significant project. Extending the period to 30 January 2023 is a lot better than the original date of 16 January, however school holidays are over this period so a lot of residents would still be on holidays. A lot more consultation needs to take place. It was also difficult to find the DA on the site as it is not listed under 'Your Say' and under the search tool you need a DA number which I spent a large amount of time searching for and can imagine other Bathurst residents would have done the same.
- Street congestion due to no parking. What will the impacts be on the surrounding streets, residents and also street safety with the increase in traffic. Where are these impacts considered in the concept and what is the reason for no parking being included in the concept DA.
- Shadowing due to hotel height and also the height of the hotel will cause a wind tunnel effect. Can't imagine this would be pleasant for the surrounding residents. This area is currently such a beautiful and sunny place to sit in Winter and enjoyed by so many residents. To turn it into the type of wind tunnel you'd find in the inner city Sydney suburbs would be such a shame.
- Artistic graphics don't allow the residents to see the true impact of the development. This, like other significant developments, should be automatically included in the Live NSW Spatial Digital Twin tool, to truly show how the development will look. It seems a little disingenuous of the developers to submit pretty pictures that don't show actual impacts on the development to surrounding buildings such as shadowing, visual impact to surrounding buildings and how the development will sit in the environment. Are council considering the requirements to add concepts such as these into the Live NSW Spatial Digital Twin platform?

Regards
Victoria Seymour
282 Piper Street
Bathurst NSW 2795

68 Morrisett Street

Bathurst NSW 2795

23/1/23

The General Manager
Bathurst Regional Council
Private Mail Bag 17
Bathurst NSW 2795



Dear Sir,

Development Application No. 2022/523

Development of Gremlin Mill - Lot 11 DP 1266801

7 Kippel Street Bathurst

Mr. K Walls - Applicant

I wish to make a submission to Council regarding the above Development Application.

I am greatly concerned regarding the car parking that will be made available for this development. For all that is proposed between apartments, cafes, wine bars, restaurants, bottle shops, distillery, markets etc hardly seems to be adequate at all. If Manilla Street is being considered as public parking for any part of this development is just not on. As this area has been a residential area for many years.

At the present time Manilla Street is stuck with people attending the nearby coffee shop which displays a sign stating 'parking is available in Manilla Street'. The people who live in Manilla Street already find it difficult to find parking in front of their house if they have been out for any reason. I doubt very much if other Bathurst residents would tolerate this.

I would also stress the danger for children in this street, whether riding their scooters or just walking down the footpath, children are unpredictable, any added

EPFS-R

traffic to this street is simply too dangerous to be even considered. There is also a child care centre close by

I also feel that dust would be a major problem in the area of Manilla Street.

Yours faithfully
J. Keston

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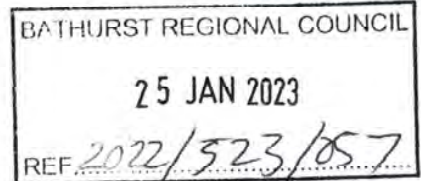
24 JAN 2023

BATHURST REGIONAL COUNCIL

Ms Yvonne Passeri
14 Keppel Street
BATHURST NSW 2795

18 January 2023

Mr R Denyer
Manager Development Assessment
Bathurst Regional Council
158 Russell Street
BATHURST NSW 2795



Dear Mr Denyer

Re: Development Application No 2022/523 – Redevelopment of Tremain Mill

Firstly, I would like to point out that I am totally in favour of new projects going ahead in Bathurst and feel that they are generally of benefit, however I have several concerns regarding the proposed redevelopment of the Tremain Mill at 7 Keppel Street, Bathurst.

I live at 14 Keppel Street, which is directly opposite the proposed development area, and I am impacted by the current ongoing traffic congestion to this area on a daily basis.

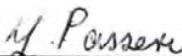
There is very limited parking in Keppel Street, between Seymour and Havannah Street, and it is not uncommon to find a car parked in my driveway blocking my own personal access. People also occasionally park on the median strip in the middle of the road.

At present, most of the activity in this area is just from people attending Doppio Café and hence I am extremely concerned about the traffic flow, and safety of others, should this area become a much larger business precinct if the project goes ahead.

There are currently five empty shops in the block above the Tremain Mill (between Bentinck and Seymour Streets), and two of the shops already located in the proposed redevelopment area are quite frequently empty. Several businesses who have previously rented these shops appear to have failed, and the clients in this area are generally only seeking coffee or visiting the hotel.

I am also apprehensive about the impact of this proposed redevelopment on nearby residential property values, including my own, and I urge you to please reconsider passing this development application.

Yours faithfully


Yvonne Passeri

Emma Castle

From: Gillian Gates <g.r.gates-hopper@hotmail.com>
Sent: Thursday, 26 January 2023 11:58 AM
To: Council
Subject: Opinions on DA 2022/523

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Hi,

Feedback for the proposed DA 2022/523 is that I strongly believe there definitely needs to be parking provided for each of the units and hotel rooms.

Otherwise, parking on the street will be grossly inadequate. People will have to walk a fair way with their luggage if they're not lucky enough to grab a parking spot near the apartments. They may even use the parking at the railway station, which is supposed to be for commuters and patrons of The Railway Museum!

The parking problem would even impact negatively on the businesses nearby, because their customers would find it very difficult to get a parking spot. For the cafes, this could easily mean that patrons would give up and go to another cafe. This wouldn't be fair.

It's sad that the silos are proposed to be demolished. They are an historic landmark for the area. Can't they be remodelled into accommodation, like has happened with lighthouses? They would be very unique, and probably attract more interest with patrons.

I also think that a grass area and cafe should be kept for customers with dogs. That is a unique niche in the market. Thank you,

My regards,
Gillian.

Emma Castle

From: l.capolino@bigpond.com
Sent: Thursday, 26 January 2023 2:09 PM
To: Council
Cc: kkmountdruitt@hotmail.com
Subject: Submission about Concept Development Application - Redevelopment of Tremain Mill/Development Application No 2022/523

Importance: High

CAUTION: This email originated from outside BRC. Do not click links or open attachments unless you recognise the sender and know the content is safe.

Antonio and Lisa Capolino
39 The Lanes
Kirkham
NSW 2570

Your Reference: Concept Development Application - Redevelopment of Tremain Mill
Development Application Number 2022/523

To The General Manager,

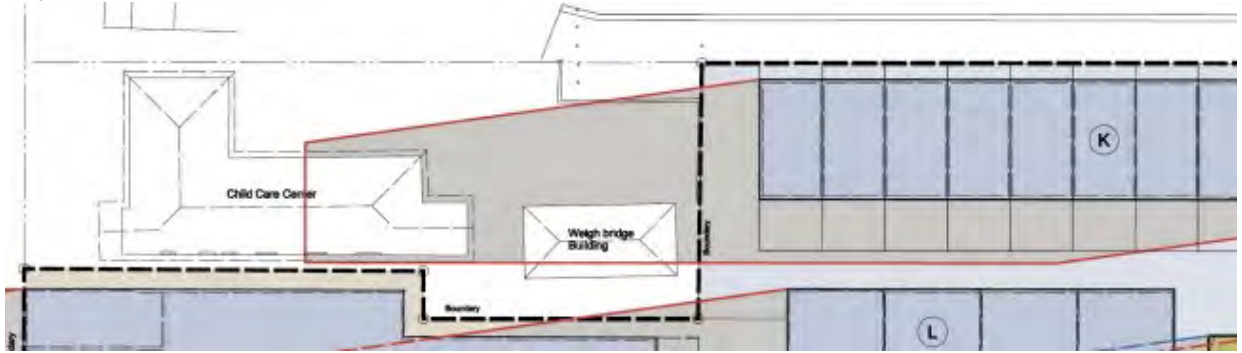
We are writing this submission as we are the owners of the property next door to the development and share two common boundaries. We are the owners of 13 Keppel St Bathurst which is the child care centre, Keppel Street child care, located next door to the Tremain Mill site and the future development.

I would like to raise the following issues with the development:

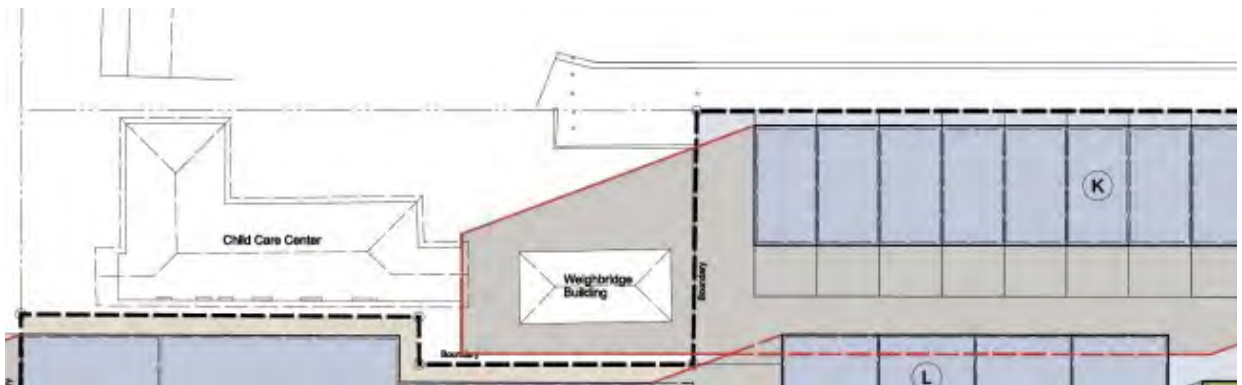
1. Looking through all of the paperwork submitted on Councils website I could not see in any way how the submission makes reference to the fact that the building next door is a child care centre and how they will accommodate our privacy, need for sun shine in the playground, noise or dust through construction etc
2. The 80 room hotel which will be positioned approx. 1.5m from our common boundary is a proposed 3 storey building. Firstly will it have windows which can look into the playground of the centre? This poses a safety issue with paedophiles- people could choose to stay at the hotel knowing it is overlooking a child care centre. Secondly we have children who attend the centre who are in foster care. These children are unable to be on social media etc as their biological parents are unable to know their location. We could have the parents staying at the hotel so they could look into the playground so that could find the location of their child. Photos of the children can be posted on social media by guests at the hotel or residents of the terraces. Therefore posing a serious safety issue to individual children and the centre Thirdly as we know domestic violence is a problem and once again we may have families who are trying to keep their location secret, having a hotel which has windows facing our centre poses real security issues. Fourthly how many windows will be overlooking our sight? Will it be common areas like stairs or landings or will it be private hotel rooms? How many hotel rooms will be able to see our child care centre?
3. The terrace building which is also proposed will be 4.5m from our common back boundary. This building also proposes the above privacy concerns as the building will have windows higher than ground level and therefore can look into our sight.
4. From the provided shadow diagrams it shows that the playground and buildings (the weighbridge building marked on the plan- is an office building which will be used by the child care centre) will be in shade for most of the day in the winter. The office building will also be used by the child care centre. The problem with the overshadowing as seen below is firstly our playground has been designed so that we have 50% shade due to our regulations. The part which does not have the overshadowing as seen at 9am has a large shade

Attachment 8.2.3.9

sail and is in shade already. Therefore, at 9am we will be in virtually 100% shade in winter. This will make our playground too cold to be used by the children. Therefore, if the playground is not getting sun then the ground/grass will be wet from the dew and with the whole playground in shade it will be really cold and wet for the children to play outside. In winter time generally the children already have limited time to play outside and do not to go outside until 9am and stay outside until 11am/12am depending on their age. Younger children get less time due to their morning sleep etc. This will severely impact the outside programs at the child care centre which plays a large part in the development of children under 5. Our Regulatory requirements are that the children play outside and this shadowing will have a major effect on the programming, development of gross motor skills and ultimately us being able to meet our regulatory requirements.



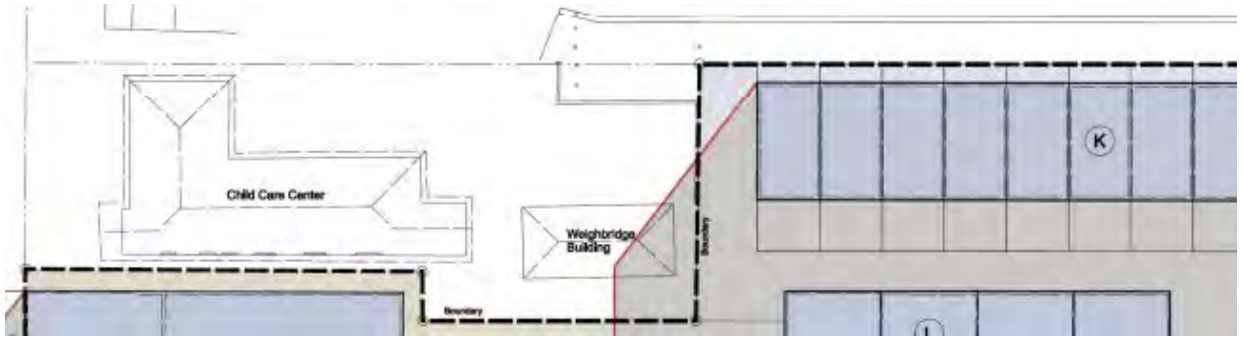
9am



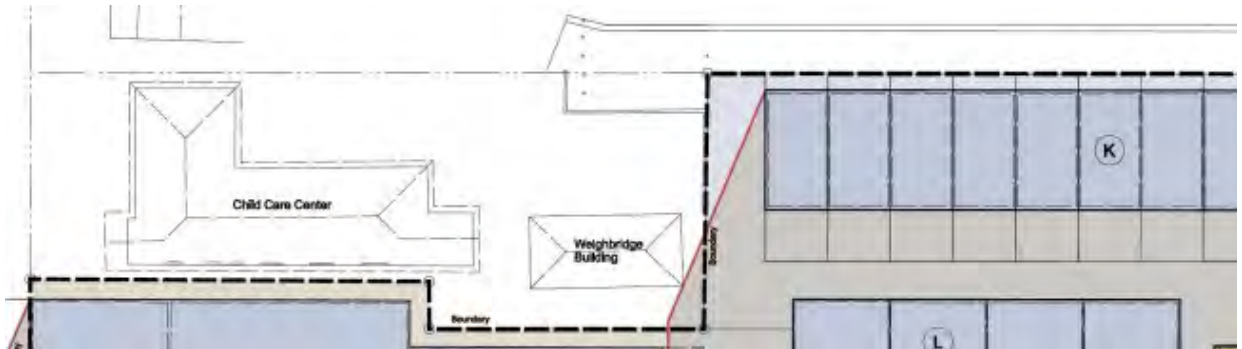
10am



11am



12 noon



1pm

<p>This area will be in shade</p>	<p>This area will be in shade</p>	<p>This area will be affected as the w with the air conditioner is the offi marked weighbridge on the plans</p>

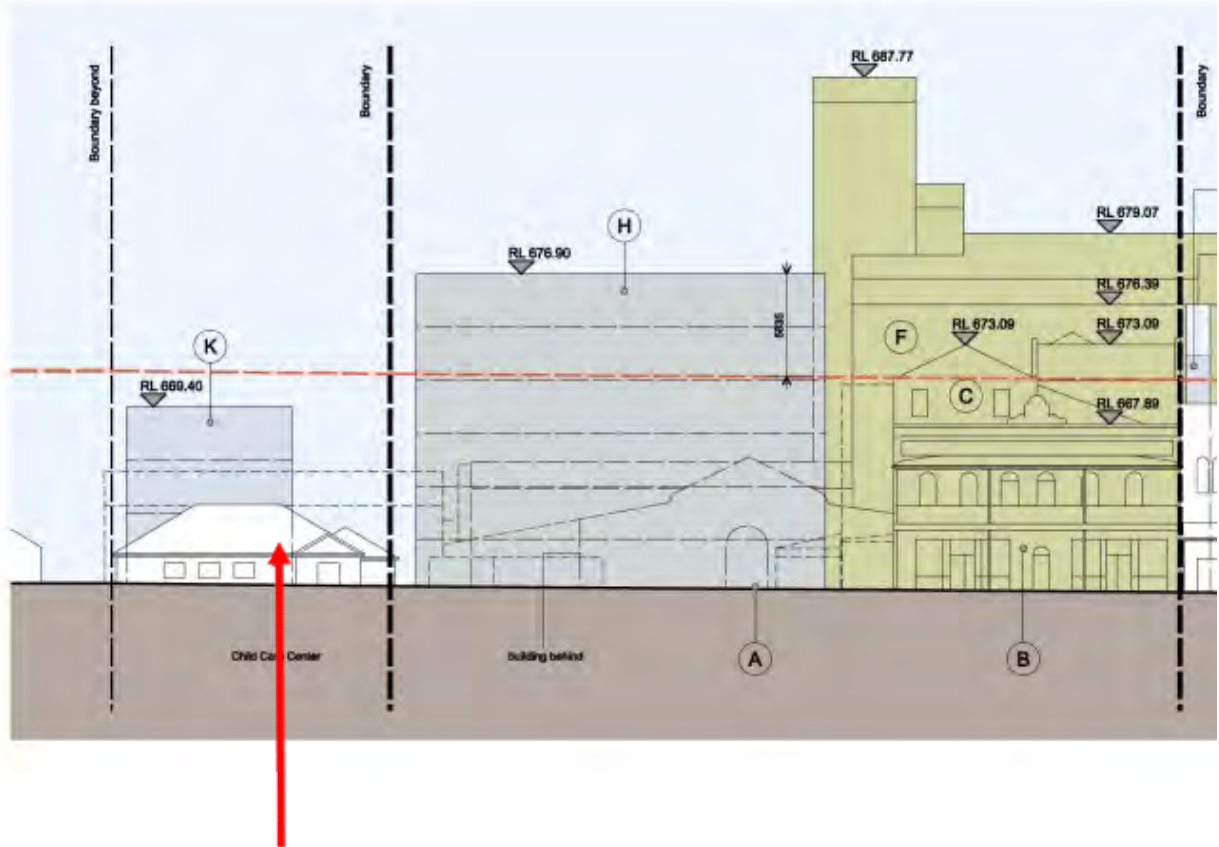
5. Below is a screen shot of the site analysis



Keppel Kindy

Attachment 8.2.3.9

7. Below is the elevations plan (7) provided in the development package.



As can be seen by this elevations diagram the child care centre will be totally over towered by this development. Building marked H (hotel) will tower over the whole of the shared boundary and the building marked (K) terraces will tower over the whole of the back boundary. The size and scale of the development is of great concern as it is located next to a child care centre.

8. We are also concerned about the demolition and construction phase and how this will impact the child care centre. The dust, disturbance, noise which this development will cause on the centre. How will these things be addressed. Children being able to sleep, learn and play outside?
9. Will this have an impact on the business? Will it impact on the value of our property? Will parents want to come to the centre seeing that there is a large construction site next door? A large over towering hotel and terraces right next to a child care centre. Will existing families want to leave?
10. Also will the development have an impact on us meeting Regulatory Requirements of keeping children safe with dust and noise during construction stage? This is not a residential development but rather a large commercial development. Will dust and noise impact our programming of outside activities therefore not meeting this large part of our requirements? Will lack of natural sun shine impact our programming of outside activities therefore not meeting this large part of our requirements? Lack of privacy? During construction will tradesman be looking into the property?

We are not opposed to some development occurring on the site but we do not want this development to be at the compromise of our child care centre. We have a right to sunlight, privacy, the ability to meet our regulatory requirements and to not be over towered by the overdevelopment of a site that is located next door to our child care centre.

We eagerly await your reply on the development and request that we receive any further information regarding the development. We can also be contacted by mobile: 0414 464 086

Yours Faithfully

Antonio and Lisa Capolino



Bathurst Heritage Matters Inc.

Bathurst's unique heritage is a key social, cultural and economic asset

The General Manager
Bathurst Regional Council
Russell St, Bathurst 2795

27 January 2023

by email

Dear Sir,

Tremains Mill site Concept DA Submission ... DA_No 2022/523

This is a submission to the "concept DA" for the Tremains Mill site in Keppel Street, now on public exhibition.

Bathurst Heritage Matters Inc. welcomes the concept of an updating and further development of the historic and heritage laden Tremains Mill precinct. There are a number of positive aspects to the proposal including public access and public/semi public spaces, and particularly the retention and re-use of the various heritage structures on the site.

We are pleased this is acknowledged as a Landmark Site in a Heritage Conservation Area. It contains heritage listed buildings, and is in a precinct with many heritage items listed at both local and State level, including items heritage listed by the National Trust. Thanks to past efforts and respectful town planning, there is a very marked cohesion in the heritage values of the Railway/lower Milltown precinct in which the Mill site sits. Therefore its redevelopment requires very careful handling and the greatest of genuine respect for its context.

We have very significant concerns about certain key aspects of the proposal.

Because of the proposed high density and heights, and lack of parking, the concept DA appears to us to be more in the nature of a tactical "ambit claim", such that when inevitably wound back it will still breach Bathurst's planning rules.

Because this is such an important and Landmark Site, we would have much preferred to see a collaborative approach so that all parties could have provided input before a concept plan was advanced to DA stage.

We are also mindful that approval of a "concept" results inevitably in a set of expectations and attitudes which creates a natural resistance to any significant amendments requested by the



Bathurst Heritage Matters is an incorporated not for profit community group formed in 2013 to advocate for better heritage outcomes in the Bathurst region

bathurstheritagematters.inc@gmail.com

www.heritagebathurst.com.au

community, but allows the developer considerable room for future amendment. Barangaroo in Sydney is a prime example of this inevitability, albeit on a very different scale.

It is proposed the development be staged and we advocate that measures be put in place to prevent “cherry picking” the stages, in particular the 6 storey hotel, and then indefinitely deferring the rest.

Building Heights

We note the project significantly breaches the 12 metre building height limit mandated in the LEP, principally through the new build of a six storey hotel, which we note is directly on the Keppel Street frontage and with no setbacks or step backs.

It is stated in the EIS that Havannah Street hotel buildings will also breach this limit.

The six storey Keppel St structure is entirely inconsistent with the rich one/two storey heritage streetscape and the heritage residential nature of the area. Also, it will impede street views to the landmark heritage silos – the only ones left in Bathurst.

There is no “social licence” for a six storey hotel development in Keppel Street. Existing nearby buildings are either two storey terraces and hotels, or single storey residences. There is no social licence to overpower the existing heritage buildings on the site and adjacent. The proposed hotel building would seriously diminish the historic Keppel Street streetscape.

It should not be permitted.

The developer may argue that the site is not viable without breaching the LEP height limit, or in developer language, the site is not “efficient”, meaning the level of profits desired cannot be obtained by abiding by existing rules. That is a financial issue for the developer who would have known exactly what the LEP and DCP provisions are before purchasing the site.

We welcome developers to Bathurst, but on the basis that they abide by and respect our planning rules which have been hard won over many decades of public consultation. These rules are not to be regarded as easily set aside just because a developer wants to suit its own interests, and we urge Council to stand firm to protect the integrity of our Central Heritage Conservation Area, and the Keppel Street “Village”.

Because of the height of the existing historic silos it seems to us that there may be scope for taller new buildings but located well within the site and towards Havannah Street, consistent with Council’s new Town Centre ideas for the centre of city CBD blocks, and we would welcome a discussion about that potential.

Design

The depictions provided in the way of artists impressions show an attempt to make the new builds look old. In other words “fake heritage”. We do not agree with fake heritage and would much prefer high quality external designs which will create “tomorrows heritage”.



Bathurst Heritage Matters is an incorporated not for profit community group formed in 2013 to advocate for better heritage outcomes in the Bathurst region

bathurstheritagematters.inc@gmail.com

www.heritagebathurst.com.au

Parking

It is clear there is huge shortfall in on site parking from the requirements of Bathurst Council – well over 100 spaces short. This arises through the proposal for 145 dwelling and hotel units, plus numerous commercial tenants.

We see in the EIS the argument that demand for onsite parking will be lower because the developer will provide shuttle bus services for hotel guests direct from Sydney, or guests will travel to Bathurst by train. Frankly this is fanciful, as all the experience shows that most people coming to Bathurst want to be able to move around the region using their own cars. Many visitors are from outside of Sydney anyway.

The developer asserts in the EIS that there are 165 car parking space “credits” because the existing site has only 13 car spaces out of the 178 calculated. With respect, that is like comparing apples with oranges. The existing use of the site is very subdued and low key, but the proposed use is highly intense, and council rules mandate that 178 car spaces are required based on that proposed occupancy. The idea of parking credits is a false premise and should be rejected.

To approve this proposal as it stands would place enormous pressure on street parking and railway parking - it would be intolerable for residents of Keppel St and Manilla St and nearby, and would be grossly unfair on them.

We note that a previous DA for a gym on the site was withdrawn because it did not provide the minimum number of car spaces. There was no “credit”.

If the parking shortfall is addressed by making financial contributions to Council, that will not resolve the very real practical on site issues. As the developer notes in the EIS ... “As a result of the site location at the southern extent of the CBD, any potential contributions would be unlikely to benefit the site directly.”

In summary, we believe the proposals for the site need a complete rethink, and so recommend to Council that this concept DA should be rejected.

Yours faithfully,

Sandy Bathgate
President
Bathurst Heritage Matters Inc.



Bathurst Heritage Matters is an incorporated not for profit community group formed in 2013 to advocate for better heritage outcomes in the Bathurst region

bathurstheritagematters.inc@gmail.com

www.heritagebathurst.com.au



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General Manager

Bathurst Regional Council

Concept Development Application - Redevelopment of Tremain Mill

Development Application 2022/523

Attention: Emma Castles

Objection

We own three properties on Manilla Street Bathurst opposite the proposed redevelopment of the Tremain Mill Precinct. We are in favour of an appropriate redevelopment.

As directly impacted neighbours, we are opposed to the plans as currently submitted to council for the following reasons:

Inadequate onsite parking:

The proposal suggests 46 parking spaces for the entire development. There is no onsite parking proposed for the 80 room hotel, the additional silo hotel rooms, the retail complex or the town houses and mews residences. This will mean on street parking will be the only option for hotel guests shop owners, shoppers, and other residents. The streets surrounding the development site do not have the capacity to support any additional parking pressure. Almost all visitors to the site as well as hotel guests and residents of the proposed townhouses will require parking. There is inadequate public parking in the vicinity of the development to take the extra pressure. The hotel should be required to have an appropriate level of onsite parking for not only its guests but also staff and visitors. The retail development needs parking and loading zones for staff and shoppers. The townhouses and mews houses should be required to have onsite parking at the same level as any other residential development in Bathurst.

It is disingenuous to suggest that hotel guests will be happy to travel on the company's shuttle bus from Sydney if it ever comes to fruition. Realistically guests will be traveling from all parts of the state not just central Sydney. Even Sydney based guests are unlikely to travel into the city to catch a bus to Bathurst when it is far quicker to just drive here.

The Traffic Management Plan draws from the Vision Bathurst 2040 report and the State Government's Transport Strategy 2056 which are Even with appropriate resident and parking provision onsite, theoretical, subject to amendment and planning for Bathurst 17 and 33 years into the future. This Development is proposed for now and should be subject to the current regulations. The Traffic Management Plan proposes to push major costs onto Bathurst Regional Council when all costs of the development should be borne by the developer.

The Development should not be considered without a proper Traffic Management Plan based on actual surveys of the site and its surrounds. On Street Parking Demand is based on surveys taken during Covid, which is an unrealistic representation of actual parking usage. The traffic movements relating to the café are based on afternoon theoretical statistics when the busy time for the café here is the morning.

The Traffic Management Plan should be required to perform proper on the ground surveys of parking and traffic on Keppel, Manilla, and Havannah Streets across all times of the day and night. Mornings and Evenings are busy with Childcare centre drop off and pick up, mornings are busy with Doppio customers, evenings and nights are busy with patrons of the Victoria Pub. Random satellite images are not a proper representation of the actual traffic and parking in the area and it is inappropriate to make decisions based on these.

Traffic on Manilla Street

Manilla Street is primarily a quiet residential street without through traffic. The street itself and the streetscape have heritage significance. The old railway line from the former mill is still seen crossing the road.

Manilla Street is not designed for the level of traffic that the proposed development will bring. Even if appropriate resident and parking provisions were required on the development site, I believe all access to the development site should be via Havannah Street and the Manilla Street entrance blocked to traffic.

The access on Havannah Street should be upgraded with appropriate traffic control measures to account for the increase in traffic movements.

Building setbacks on Manilla Street

Sketches of the proposed townhouses show them set back from the street with gardens and trees between them and the street. The proposed plan shows the townhouses located on the edge of the property approximately 2.5 metres from the street. Given the set back of the houses on the other side of the street it would be appropriate to require the set back to be the same.

The redevelopment of the Tremain Mill Precinct is a once in a lifetime opportunity to bring a vibrant and heritage appropriate new development to the site and Bathurst as a whole. It is critical that Bathurst Council makes decisions that will fit into the city in the future. This proposal is an overdevelopment of the site and puts impacts onto neighbours that should be contained within the site.

Sally Newton-Chandler

PO Box 111

Bathurst NSW 2795

Emma Castle

From: Peter Cole <pcole2795@outlook.com>
Sent: Friday, 27 January 2023 4:34 PM
To: Council
Subject: TREMAINS Mill redevelopment Concept DA No.2022/523

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Bathurst Regional Council
158 Russell Street
Bathurst NSW 2795

Att: General Manager
(Mr D Sherley)

Hello David – please allow me to voice my disapproval of the proposed planning for redeveloping the Tremains Mill Precinct.

Business was going along quite well as it was, when Steve Birrell and Glenda Deans were involved – even with the green-scape that was in place and trees the way they were maturing. Along with being so family friendly.....it was a blessed site/sight.

I do hope (if this proposal goes ahead as seen/planned) that the maples are saved and maybe even relocated to the works at Centennial Park.

With regards building height - will the silos stand secure for such development, after all these years and could overshadowing from all tall inclusions affect Keppel St Kindy and nearby residences.

Please also, what is to become of the Historic Weighbridge that stands there, or could this be relocated over to The Bathurst Rail Museum as a feature of what was once used to weigh grain wagons, that were despatched from the Tremains Siding, back in the days when I managed the freight account for Tremains at the former Bathurst Railway Goods Office, that stood inside the lower Piper St gates. I was the last freight accounts and claims clerk Bathurst Station & Freight Centre had with the restructure of NSW Freight and offices.

With regards traffic flow.

The bottom end of Keppel St is congested enough when heavy traffic is moving. Heavens knows where the loading zones will go and then there is the islands to think of. But they can be gotten around, as no doubt with the ongoing maintenance they require - they'll get pulled up, apart from the light standards & dome base which is all heritage.

Add, residents hardly get to park their own vehicles long enough (where applicable) in lower Keppel St and is chaos during the day.

Residents cant park at front or close nearby, due to Loxley House Medical and patients, across the road in former Fengore Plumbing Shop where Austloans now occupies and at best have 7 or 8 cars from employees parking during the day – all day.

Add to that I thought I read where a wine/spirit outlet would be included in the 28 shops.

Attachment 8.2.3.9

Real nice, next door to Keppel St Kindy, plus it would be double standards to allow it, as a former proposal went to Council to retain a bottle shop in Keppel St (where the Dulux Paint Shop is) in the next block up and Council disallowed it, due to other liquor outlets in the nearby area and we can also now add to that, with the completion of Bathurst City Centre with BWS.

If there isn't already enough vacant shops in this City - I guess more in this enclosed area won't be a worry, as they'll hardly be seen unless visiting the "complex"/precincts.

Street parking at night will bring some temptations from those who love to break into vehicles, as this has happened a couple of times where NSW Police Forensics were called. My wife's Subaru car was one of them – one weekend, along with an attempted break-in the same night in a next door residence.

Look out Bathurst Commuter Car Park as a quick fix for a parking spot!

Thank you kindly for giving me the opportunity to also express situations in this Milltown area.

Kind regards

Peter J Cole (Bathurst resident and ratepayer)
34 Keppel St
Bathurst, NSW, 2795



PO Box 201
Bathurst NSW 2795
febs4@bigpond.net.au
www.nationaltrust.org.au

Friday 27th January 2023

Mr David Sherley
General Manager
Bathurst Regional Council
Russell Street, Bathurst

**Re: DA No 2022/523 Proposed Development: Concept Development Application –
Redevelopment of Tremain’s Mill Premises: Lot 11 DP 1266801, 7 Keppel Street Bathurst**

Dear David,

The local Branch of the Trust appreciates the opportunity to comment on this Concept Development Application

The NT Branch supports the vision of activating this site for retail, artistic and residential uses. The developers are to be congratulated for re-imagining this iconic and important heritage site. Notwithstanding, the local branch has several significant concerns with this proposal. They are in no special order:

Height of the Proposed Hotel

The requested variations to the height limit will dwarf the existing buildings in Keppel Street and Manila Street with their bulk and overshadow the existing Childcare Centre in Keppel Street.

There is no apparent public benefit or “social licence” in allowing the proposed hotel development in Keppel Street to overpower the existing, recently restored heritage building, nor the Childcare centre. Such a large building would detract from the long established and iconic Keppel Street streetscape including the heritage listed terraces opposite.

Recommendation: The hotel building should be no higher than the existing buildings in Keppel Street to maintain the streetscape and amenity for residents and visitors alike. Iconic views from and along Keppel Street to the Tremain’s Mill complex should not be blocked, compromised or interrupted.

Overshadowing / Overlooking

The proposed terraces in Manila Street will overshadow the childcare centre from 9.00 a.m. to 2.00 p.m. in Winter, (see shadow diagrams submitted with the application). Should the terraces be approved in the position proposed, the Childcare centre may not be able to meet the Commonwealth guidelines for provision of play areas and sunlight. Childcare places are at a premium and the potential loss of spaces would impact local families.

The Bathurst & District Branch of the National Trust - A Local Voice for Heritage

The National Trust of Australia (New South Wales) ABN 82 491 958 802



The proposed hotel would also overlook the Childcare centre and impinge upon the privacy of children, their parents and childcare workers.

Recommendation: Development along Manila Street should be single storey to prevent overshadowing adjoining properties, particularly the Childcare centre. One storey would also maintain the present height of pre-existing heritage homes in this precinct. Privacy of children is paramount in relation to the proposed hotel development.

Parking / Traffic

Manila Street is a narrow local road with access only to Russell Street. The lane way to Keppel Street is ingress only and increasing the number of vehicles using Manila Street sets up a future traffic conflict.

The Traffic report assumes that no car parking spaces are need for an 80-bed motel as occupants will come from Sydney only or rely solely on public transport. With three boarding schools, Charles Stuart University, motor racing weekends, and a strong heritage tourism economy, occupants will come from around the state and interstate by private vehicle as well as public transport creating traffic and parking chaos around the site.

The likely result of this is that the railway carpark, built for commuters and visitors to the Rail Museum, will be full of vehicles belonging to hotel guests. Parking in Havannah Street presently used for patrons of the busy Victoria hotel will be at a premium.

Previous owner of the Tremain's site, Mr. Steven Birrell, had plans for an onsite Gym refused as a result of the stringent application of Council parking requirements. The same stringent approach should be equitably applied to this application.

Recommendation: Car parking spaces must be provided onsite for hotel occupants and potential retail and arts patrons.

Overdevelopment of the Site

The Branch believes that the full list of proposals represents an overdevelopment of the site. Whilst the proposed creation of public spaces and multiple users including bakery, restaurant, coffee shops, outside tables, art exhibitions, arts workshops, and a public plaza are all desirable and admirable additions, these will not make up for the loss of the present ample open public space and the present amenity of the site for the local community. Overdevelopment will lead to a loss of sight lines, particularly to the silos, from almost all directions. This location is not in a hemmed in, high rise city block, but in a country town with wide streets and expansive views.

Recommendation: Fewer buildings on site and more public access to the open spaces that this would create and to the views and heritage attractions of the precinct.



Fake' Heritage

Whilst the Branch appreciates that the drawings included in this proposal of possible future buildings on site are only suggestions, there is a concern that the any new buildings, of the type shown could be interpreted as faux heritage, detracting from the genuine heritage buildings adjacent.

Recommendation: Any new buildings and the materials used in their construction, should be sympathetic to the style, construct, height, bulk and scale of the precinct but should not be faux heritage in style or intent.

Milling Museum

The previous owners plan for a Milling Museum with support from Council and community appears to have been put on hold. This would be a great loss to the shared vision of a museum led Railway precinct and further tourist attraction for Bathurst. There needs to be an ongoing recognition of the whole flour milling heritage of the precinct. Space needs to be set aside for telling the story of the mill through state-of-the-art interpretive signage and displays.

Recommendation: The Tremain Mill's history, fabric, equipment and artefacts need to be retained and celebrated for access by the local community and visitors to the city. Council should encourage a continuation of the Milling Museum concept with the new owners.

Conclusion

Whilst there is merit in a visionary reimagining of the Tremain's precinct, the request for the variation in LEP height limit for the site should not be approved as the proposed buildings will dominate the heritage assets adjacent.

It is important to retain the visibility of, and sight lines to, the iconic heritage elements of the site.

The proposed development for Keppel Street should be limited to the height of the existing heritage buildings. The proposed Manila Street development should be redesigned as single storey residences adjoining the Childcare Centre to prevent seasonal overshadowing of the play area.

The parking outlined in the Development Application supporting documents is inadequate and based on a false assumption that the only users of the hotel will be from Sydney CBD and will come to Bathurst by public transport.

The concept drawings and plans suggest faux heritage buildings and an overdevelopment of the site.

The Milling Museum concept should be vigorously pursued.

The Branch understands that separate and certainly more specific development applications will be submitted for each future proposed building or development on the site. We therefore reserve our position on these buildings until further details of each are supplied.

The Bathurst & District Branch of the National Trust – A Local Voice for Heritage

The National Trust of Australia (New South Wales) ABN 82 491 958 802



Our significant concerns are with the overall and broad concept as submitted and exhibited. Further, we are uncertain as to the legal or temporal standing of a 'concept development application' going forward.

Thanking you once again for the opportunity to comment and reflect upon this proposal. WE would be happy to be involved in any community consultation or discussion with the owners and Council as to the future possibilities of this important and iconic precinct.

Yours Sincerely,

Wayne Feebrey, Chair,

Bathurst and District Branch of the National Trust

The Bathurst & District Branch of the National Trust – A Local Voice for Heritage

The National Trust of Australia (New South Wales) ABN 82 491 958 802

Page 4 of 4

Submission to DA 2022/523 (Tremains Mill site Concept)

**Submitted by Chris O'Rourke (resident of Bathurst)
27th January 2023.**

The proposal, while still at concept stage, has a number of elements which would enhance the heritage characteristics and general amenity of lower Keppel Street.

It also has elements which would significantly detract from the built heritage and general character of that part of Bathurst.

On the positive side the concept designs present an appealing and vibrant area. The integration of extant heritage structures on the site is, prima facie, commendable. It should be pointed out that it appears to be laneways lined by trees, and it remains to be seen whether or not the intentions of the Landscape Precinct Plan will be realized. There are lots of “pretty pictures”. They could deceive some respondents who might interpret them as a real representation of the “final design”.

This has become, in the view of this writer, a trend in recent years: to present a rose coloured view of the public domain where people gather to eat and socialize. There is nothing wrong with this but we must let ourselves be distracted by the pretty pictures.

There are, however, significant problems with certain aspects of the application.

Firstly the attempt to match the heritage values of the existing buildings the designs fall short in terms of materials and colour thus losing the overall look and feel of the heritage precinct (lower Keppel and Milltown).

Secondly, Buildings H and M are completely out of scale with the existing buildings, notwithstanding the presence of the silos.

In fact the developers have ignored the 12 metre building height mandated by the LEP, in proposing a six storey hotel right on the Keppel Street frontage. The proposal contains buildings which will overwhelm what is probably THE “landmark site” in the Bathurst CBD. Views to and from these heritage items, notably the famous heritage silos, will be significantly compromised. Streetscapes are integral to heritage areas like this and the plan completely ignores their importance.

While I have no objection in principle to what is referred to as “fake heritage” it is difficult to do it properly and it would be preferable for this site to retain the line, form and colour of the existing precinct: “Bathurst red brick”.

The provision of parking spaces is problematic.

It is this writer’s view that BRC must, as a matter of urgency revisit its traffic and parking policies through a review, involving comprehensive community consultation. In particular it should focus on the appropriateness of the mandatory minimum car parking requirements and the future implementation of active and public transport. The belief, that people in places like Bathurst, now need and will continue to need into the distant future, to use their cars as much as they do, is widely and very strongly held, despite the evidence that this is not so.

We must reduce car dependency for a range of reasons including: increased carbon emissions, public amenity, public health, overuse of limited resources, equality of opportunity and so on.

The 12 car parking spaces **not** undercover (see p.3 of the Concept Master Plan) do not show any provision for tree plantings and thus reinforce the comment above regarding the possible misinterpretation of Landscape Precinct Plan.

Until the people of Bathurst can come to terms with their obsession with parking as close as possible to their desired destination, the provision of parking in this proposal will be regarded as deficient.¹

Were this DA to be approved it would represent a significant over-development. It would not be in the interests of the people of Bathurst.

Thus it is recommended that BRC reject this concept DA as is, and ask the developers to revisit their plans and to significantly scale back the size of the buildings and their design so that they compliment the existing built heritage.

End.

¹ This writer is a car owner and user and is not anti-car.

Emma Castle

From: Jed Coppa <jcoppa@gmail.com>
Sent: Saturday, 28 January 2023 11:57 AM
To: Council
Subject: Development Application (10 - 2022 - 523 - 1)

CAUTION: This email originated from outside BRC. Do not click links or open attachments unless you recognise the sender and know the content is safe.

To the General Manager,

I would like to express my support for the DA Redevelopment of Tremain Mill, DA (10-2022-523-1). I have seen on social media people attempting to drum up support against the project and so I would like to make my support for it known.

It is wonderful to see forward thinking plans to improve the diversity and density of land use in the Bathurst CBD. This site will help improve the Keppal Street heritage area and is adjacent to the train station, which will hopefully continue to see improved service.

If the project can successfully navigate restoring the heritage value of the site whilst giving it a functional, contemporary use it will no doubt become an asset to the town.

I hope the project gets the go-ahead, and the naysayers, who would like to see Bathurst remain perpetually in the 1970s do not win out.

Regards,

Jed Coppa
25 Tobruk Cres, Orange

Emma Castle

From: helana donaldson <helanadonaldson@yahoo.com.au>
Sent: Saturday, 28 January 2023 1:11 PM
To: Council
Subject: Proposed 6 level hotel on Tremain site.

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I am writing to object to the placement of this building so close to the footpath and frontage of the adjacent premises. This area is a first impression of Bathurst for train travellers and to show such disrespect for the existing ? Heritage precinct is disappointing.

The placement further to the rear of the property where the Mill buildings are would be preferable. Also where is the off street parking?

Helana Donaldson

[Sent from Yahoo Mail on Android](#)

Emma Castle

From: Tracy Sorensen <tsoren@tpg.com.au>
Sent: Saturday, 28 January 2023 5:02 PM
To: Council
Subject: Development Application (10 - 2022 - 523 - 1) Redevelopment of Tremain Mill

The General Manager

Bathurst Regional Council

Dear Sir,

I wish to comment on the proposed redevelopment of Tremain Mill DA (10 - 2022 - 523 - 1).

I have been a resident of South Bathurst for almost 20 years. Over the years I have spent many hours in the cultural precinct of Keppel Street between the Railway Station and Bentinck Street. For me this part of Bathurst is, along with Kings Parade and Machhattie Park, crucial to a sense of place.

The large silos visible from Keppel and Havannah Streets give the town its particular character and are a wonderful reminder of its history.

I am keen that we retain the sense of place in this part of town. Commercial developments in the area need to honour the heritage, history and current use and value of the area. The existing Local Environmental Plan specifies how we might achieve this, particularly in relation to height restrictions on new buildings.

The proposed development is simply too big for the site. It will overshadow the childcare centre next door and overwhelm the existing sense of scale at the railway end of Keppel Street. It is not in the best interests of the Bathurst community to overturn our own LEP in favour of a developer's interests. Instead, we must insist that development accord with our own goals as a community.

The fact that some buildings on site already exceed the general height restrictions should not be considered an invitation for new buildings to break existing height restrictions. The Mill buildings are magnificent landmarks in the Bathurst cityscape and should stand well taller than new developments, in order to keep an existing sense of place and scale. To crowd them in with oversized new development will take something essential from the existing sense of place down that end of Keppel Street.

Yours sincerely,

Tracy Sorensen
 Torch Street
SOUTH BATHURST NSW 2795

[Please do not publish my street number]

Emma Castle

From: Glenda Burke <gdburke50@gmail.com>
Sent: Sunday, 29 January 2023 11:28 AM
To: Council
Subject: DA NO 2022/523 Tremains Mill

CAUTION: This email originated from outside BRC. Do not click links or open attachments unless you recognise the sender and know the content is safe.

Dear Councillors,

As a local citizen and rate payer, am writing to register a rejection to the proposed development at Tremains Mill (DA 2022/523). I do so on the following grounds:

a) Size of the proposed development.

As it is, the size (both in proportions and number of dwellings) proposed is out of keeping with the surrounding area and will have a serious impact on the amenity of the immediate buildings including overshadowing, visual obstruction and general ambience. At the moment the Doppio Green provides a welcome area of green space for locals in the immediate area and visitors as evidenced by its popularity. This will disappear and seriously impact those who live in the locality.

b) Parking

The proposal does not make adequate provision for parking for the number of dwellings and commercial premises. As a resident of Piper St at the railway end I can foresee our streets jammed with visitors to the site, as well as those who reside or work there, who have no other choice but to use local streets for parking.

c) Heritage

The block surrounding the mill is composed of heritage buildings and street lights, an eclectic variety and mix in its buildings, and the recently restored and impressive railway precinct and museum. This presents an interesting, unique and authentic experience of Bathurst.

Certainly, much more could be made of the precinct (the Doppio Green and art space are a great start) and imaginative architecture, lower building heights and more recognition of the heritage values of the area give it great potential.

There are areas in Bathurst much more suited to the type of development proposed by this DA (corner of Bentinck and Howick for instance). We cannot afford to make the mistakes of Barangaroo and Parramatta where heritage and local amenity have been sacrificed to development demands.

I hope you consider this objection, and those of other concerned residents, seriously and reject the proposal as it stands.

Regards,
Glenda Burke
42 Piper St, Bathurst
0405008635

Emma Castle

From: Bathurst Regional Council <council@bathurst.nsw.gov.au>
Sent: Sunday, 29 January 2023 11:43 AM
To: Council
Subject: Contact Bathurst Regional Council

CAUTION: This email originated from outside BRC. Do not click links or open attachments unless you recognise the sender and know the content is safe.

Name

Dr Jim Blackwood

Email

jpb49@bigpond.com

Contact Phone Number

0488460788

Address if Applicable

27 Stewart Street Evans Plains

Subject

Tremains Mill proposed development

Message

Higher density residential development in lower Keppel Street will bring substantial benefits to Bathurst, however the current proposal must be rejected. The multi story hotel height, scale and proximity to heritage buildings is totally inappropriate. The heritage skyline must be preserved. Lack of parking is currently a major issue in the area. Appropriate parking spaces must be provided within the development. There are currently large numbers of vacant retail premises in Bathurst. It is not appropriate that more are approved. In summary, the residential development is desirable. The hotel must be totally rejected. More parking must be provided, and the number of retail spaces reassessed.

File Upload

Wiradjuri Country
W: www.bathurst.nsw.gov.au



Bathurst Regional Council acknowledges the Wiradjuri People as the Traditional Custodians of the land in the Bathurst Region. We pay respect to Elders, past, present, and emerging and acknowledge all Aboriginal and Torres Strait Islander people who live, work and play in the Bathurst region.

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Emma Castle

From: Andrew & Bronwyn Elliott <angeyboy@live.com.au>
Sent: Sunday, 29 January 2023 3:52 PM
To: Council
Subject: Tremain Mill Development
Attachments: IMG_8022.JPG; IMG_8023.JPG

CAUTION: This email originated from outside BRC. Do not click links or open attachments unless you recognise the sender and know the content is safe.

Dear Council

No, no, no, no, no!! You cannot allow the Tremain Mill Development to go ahead without any on-site parking.

This is absolutely ludicrous that 90 hotel rooms and 28 shops are not having any parking supplied, in a high residential area.

My parents live in Lower Russell Street, and it is already difficult to get a parking spot when visiting, as a lot of homes in the area do not have off street parking available. Residents should have the luxury of at least being able to park in front of their own homes. If these spots are then taken by hotel guests, shoppers and staff, where are the residents expected to park???

Isn't it the responsibility of the developer to ensure that they provide adequate parking for what they are proposing? Isn't it responsibility of Bathurst Regional Council to ensure that Bathurst 's rate-paying resident's needs come first? There is not ample capacity for an additional 200 parking spots in this area. Besides the Victoria Hotel, the Tremain Mill area, Keppel Street Kindy, and a few business in Russell Street, this area is all residential.

And what if I were a hotel guest? I wouldn't be impressed if my vehicle wasn't secure in on-site parking, and I had to walk a distance, with luggage, from my vehicle to my hotel room.

What shops will the new development include? Will there be shopping trolleys left in front of homes or in gutters because people had to walk so far back to their car, that they couldn't be bothered walking back to the shops to return the trolley?

It is bad enough when there are events at Carrington Park, any street parking is taken for hours on end, often with patrons so desperate for parking , they park over residential driveways. No consideration given to residents at all. Please do not allow this to become a common occurrence on a daily basis.

Nor should the additional parking at the Bathurst Railway be used for this development. Council had the ability to recognise with 2 daily train services to Sydney and return, this parking space was a necessity for travellers.

I have attached a couple of photos that I took on Saturday evening of lower Russell Street. This shows the typical amount of street parking required by residents in this area.

Please do not let this development go ahead without on-site parking. Please consider Bathurst residents of this area.

Kind regards

Bronwyn Elliott

From: Jim Blackwood <jpb49@bigpond.com>
Sent: Sunday, 29 January 2023 5:11 PM
To: Robert Taylor
Cc: Council
Subject: Tremains proposal

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Robert, my thoughts on this proposal;

Higher density residential development in lower Keppel Street will bring substantial benefits to Bathurst, however the current proposal must be rejected.

The multi story hotel height, scale and proximity to heritage buildings is totally inappropriate. The heritage skyline must be preserved.

Lack of parking is currently a major issue in the area. Appropriate parking spaces must be provided within the development.

There are currently large numbers of vacant retail premises in Bathurst. It is not appropriate that more are approved.

In summary, the residential development is desirable. The hotel must be totally rejected. More parking must be provided, and the number of retail spaces reassessed.

Jim Blackwood

Sent from my iPad

From: Jim Blackwood <jpb49@bigpond.com>
Sent: Sunday, 29 January 2023 5:04 PM
To: Kirralee Burke
Cc: Council
Subject: TREMAINS DEVELOPMENT

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Kirralee, my brief thoughts re proposed development;

Higher density residential development in lower Keppel Street will bring substantial benefits to Bathurst, however the current proposal must be rejected.

The multi story hotel height, scale and proximity to heritage buildings is totally inappropriate. The heritage skyline must be preserved.

Lack of parking is currently a major issue in the area. Appropriate parking spaces must be provided within the development.

There are currently large numbers of vacant retail premises in Bathurst. It is not appropriate that more are approved.

In summary, the residential development is desirable. The hotel must be totally rejected. More parking must be provided, and the number of retail spaces reassessed.

Jim Blackwood

Sent from my iPad

From: Jim Blackwood <jpb49@bigpond.com>
Sent: Sunday, 29 January 2023 5:06 PM
To: Ben Fry
Cc: Council
Subject: Tremains development

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Ben, my brief thoughts re proposal;

Higher density residential development in lower Keppel Street will bring substantial benefits to Bathurst, however the current proposal must be rejected.

The multi story hotel height, scale and proximity to heritage buildings is totally inappropriate. The heritage skyline must be preserved.

Lack of parking is currently a major issue in the area. Appropriate parking spaces must be provided within the development.

There are currently large numbers of vacant retail premises in Bathurst. It is not appropriate that more are approved.

In summary, the residential development is desirable. The hotel must be totally rejected. More parking must be provided, and the number of retail spaces reassessed.

Jim Blackwood

Sent from my iPad

From: Jim Blackwood <jpb49@bigpond.com>
Sent: Sunday, 29 January 2023 5:07 PM
To: Graeme Hanger OAM
Cc: Council
Subject: Tremains proposal

CAUTION: This email originated from outside BRC. Do not click links or open attachments unless you recognise the sender and know the content is safe.

Graeme, my brief thoughts re proposal;

Higher density residential development in lower Keppel Street will bring substantial benefits to Bathurst, however the current proposal must be rejected.

The multi story hotel height, scale and proximity to heritage buildings is totally inappropriate. The heritage skyline must be preserved.

Lack of parking is currently a major issue in the area. Appropriate parking spaces must be provided within the development.

There are currently large numbers of vacant retail premises in Bathurst. It is not appropriate that more are approved.

In summary, the residential development is desirable. The hotel must be totally rejected. More parking must be provided, and the number of retail spaces reassessed.

Jim Blackwood

Sent from my iPad

From: Jim Blackwood <jpb49@bigpond.com>
Sent: Sunday, 29 January 2023 5:08 PM
To: Jess Jennings
Cc: Council
Subject: Tremains proposal

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Jess, my brief thoughts re the proposal, = disaster..

Higher density residential development in lower Keppel Street will bring substantial benefits to Bathurst, however the current proposal must be rejected.

The multi story hotel height, scale and proximity to heritage buildings is totally inappropriate. The heritage skyline must be preserved.

Lack of parking is currently a major issue in the area. Appropriate parking spaces must be provided within the development.

There are currently large numbers of vacant retail premises in Bathurst. It is not appropriate that more are approved.

In summary, the residential development is desirable. The hotel must be totally rejected. More parking must be provided, and the number of retail spaces reassessed.

Jim Blackwood

Sent from my iPad

From: Jim Blackwood <jpb49@bigpond.com>
Sent: Sunday, 29 January 2023 5:09 PM
To: Andrew Smith
Cc: Council
Subject: Tremains proposal

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Andrew, my thoughts re this proposal;

Higher density residential development in lower Keppel Street will bring substantial benefits to Bathurst, however the current proposal must be rejected.

The multi story hotel height, scale and proximity to heritage buildings is totally inappropriate. The heritage skyline must be preserved.

Lack of parking is currently a major issue in the area. Appropriate parking spaces must be provided within the development.

There are currently large numbers of vacant retail premises in Bathurst. It is not appropriate that more are approved.

In summary, the residential development is desirable. The hotel must be totally rejected. More parking must be provided, and the number of retail spaces reassessed.

Jim Blackwood

Sent from my iPad

From: Jim Blackwood <jpb49@bigpond.com>
Sent: Sunday, 29 January 2023 5:10 PM
To: Ian North
Cc: Council
Subject: Tremains proposal

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Ian, my thoughts on this proposal;

Higher density residential development in lower Keppel Street will bring substantial benefits to Bathurst, however the current proposal must be rejected.

The multi story hotel height, scale and proximity to heritage buildings is totally inappropriate. The heritage skyline must be preserved.

Lack of parking is currently a major issue in the area. Appropriate parking spaces must be provided within the development.

There are currently large numbers of vacant retail premises in Bathurst. It is not appropriate that more are approved.

In summary, the residential development is desirable. The hotel must be totally rejected. More parking must be provided, and the number of retail spaces reassessed.

Jim Blackwood

Sent from my iPad

From: Jim Blackwood <jpb49@bigpond.com>
Sent: Sunday, 29 January 2023 5:12 PM
To: Warren Aubin
Cc: Council
Subject: Tremains proposal

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warren, my thoughts on this proposal;

Higher density residential development in lower Keppel Street will bring substantial benefits to Bathurst, however the current proposal must be rejected.

The multi story hotel height, scale and proximity to heritage buildings is totally inappropriate. The heritage skyline must be preserved.

Lack of parking is currently a major issue in the area. Appropriate parking spaces must be provided within the development.

There are currently large numbers of vacant retail premises in Bathurst. It is not appropriate that more are approved.

In summary, the residential development is desirable. The hotel must be totally rejected. More parking must be provided, and the number of retail spaces reassessed.

Jim Blackwood

Sent from my iPad

Emma Castle

From: Neil and Cathy Dethick <neilandcathydethick@gmail.com>
Sent: Sunday, 29 January 2023 7:53 PM
To: Council
Subject: Objection to Proposed DA 2022/523 - Tremain's Mill.

CAUTION: This email originated from outside BRC. Do not click links or open attachments unless you recognise the sender and know the content is safe.

For attention of the General Manager.

Dear Sir.

I wish to submit my objection to Proposed DA 2022/523 - Tremain's Mill.

Initially, upon viewing the published documents, I was rather supportive of the concept. It was after closely reading the details of scale and parking, however, that my position has changed, and I feel compelled to express my opposition to this development in it's proposed form.

My objections are due to the following issues:-

-Proposed height breaches the adopted Local Environment Plan limit of 12 metres. Any new development on this site must comply with this height limit to gain my support. The fact that heritage buildings on the street exceed this limit is no justification for any new development exceeding 12 metres.

-The proposed amount of car parking established to cater for this new development is woefully insufficient in the context of the area and falls well short of accepted standards. Again, excuses given by the developer regarding previous development provisions of parking and their interpretation of what is "adequate" ought not be entertained by Council. Council must act in the best interests of residents and the local community. Lower Keppel Street is already problematic from a parking perspective. Adoption of this DA would worsen this situation and exacerbate the already significant safety risks associated with pedestrians crossing the road.

-The proposed development would totally disconnect heritage elements, listed in the LEP as a group, from each other. It would visually separate the "Invincible Flour" concrete verge sign from the rest of the Tremain's Mill group of buildings, causing a loss of context. As the owner of the Tremain home, "Kallara", at 12 Keppel Street, and directly across the road, I feel that all these elements are best retained as a group, as each element provides heritage context for the others.

Yours sincerely,
Catherine Dethick.

Emma Castle

From: keith illingworth <keithillingworth@yahoo.com.au>
Sent: Sunday, 29 January 2023 8:01 PM
To: Council
Subject: Re Tremain Mill proposal to build 90 hotel rooms and 28 shops

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Dear Sirs, the above proposal without adequate onsite parking is an unacceptable development. As the previous owner of Inspirations Paint located at 51 Keppel St I know that parking is often difficult in that area. The proposed development will only exacerbate that problem and due to parking time limits in Keppel St will probably make parking in surrounding residential streets extremely difficult for people who live in those streets. I believe such large scale developments in our CBD will only increase traffic congestion and cause parking issues without any significant long term benefit to Bathurstians. I hope Council will not approve this development.
Yours Faithfully Keith Illingworth

29 January 2023

The General Manager
Bathurst Regional Council
Via email to council@bathurst.nsw.gov.au

Dear Sir,

Submission regarding Concept DA 2022/523

It is pleasing to see renewed interest in the Tremains Mill site and we commend the new owners on bringing forward the Concept DA for consideration. The logic behind the proposed mixed-use precinct is sound from a broad urban planning concept, however, the proposed GFA, height levels and limited parking present a problematic concept for such historical location at the “gateway” to Keppel St and the main transport hub for Bathurst.

HEIGHT, SIGHT LINES AND SHADING

The proposed height of the Building H (hotel) and Building M (car parking and residences) does not complement or enhance the site or locale. Obviously it is at odds with the 12 M height limits of the BRLEP 2014. Council has already recently considered changing the height limits for the Bathurst Integrated Medical Centre (BIMC) site. We think, generally, Bathurst residents understand that the scale necessary for a hospital/medical facility, the criticality of such a facility for the city and the uniqueness of that proposed site could justify a change to the height limit as a one off consideration to get such a development moving forward.

The Tremains/Mill site and Keppel St Precinct is a significantly more critical heritage area than the BIMC site and all developments within that precinct MUST complement the existing structures of the streetscape in the area. Using the height of the existing, historical industrial structures as a benchmark on the site should be irrelevant in current the planning context. There is nothing critical or unique about a hotel development. Heights of buildings forming the streetscape within the precinct should inform any building height recommendations. Further, Building H exhibits very poor design with no graduated frontage to Keppel St, at a minimum the building should step back from a two storey frontage to Keppel St. None of the proposed buildings for this site offer any interesting, articulated facades, the concept designs aim to maximise floor space only.

The existing Child Care Centre, next door, will be overlooked by a building some 18 metres high, and with many hotel room windows. This is unacceptable, children going about their play in a Child Care setting should not be overlooked by hotel rooms and this presents a privacy and child security risk.

One of the significant values and amenity of the site currently are the sight lines from Keppel and Havannah Sts into the site and its heritage assets. These new buildings completely obliterate both near and long distance sight lines as people move around the Keppel St/heritage railway precinct. Building H proposed height ensure that the proposed open space, especially the Central Courtyard will be in shade all through winter. Rather than making site attractive outdoor spaces (as they are currently) the constrained open spaces will be cold and uninviting for much of the year. The shading issue extends to existing residential dwellings on the opposite side to Havannah St impinging on the amenity of those existing homes, and possibly their value.

HOTEL CAPACITY AND CAR PARKING

99 hotel rooms are proposed – with no commensurate car parking? On street parking is considered an acceptable outcome by the proponent. The proponent is looking for a parking offset of a “notional” 165 car parking spaces (Traffic Assessment Report). This is based on the site having existing GFA for business and office premises and entertainment and function centre capacity. This request is illusionary as the bulk of GFA on the site is underutilised, not fit for purpose industrial era storage space.

The Traffic Assessment Report also argues a 70 % occupancy rate for the hotel. As an average this may be correct (and we wish them luck with this occupancy rate) but Bathurst has many race/event weekends/weeks in each year where hotel and motel capacity will at 100% capacity. This hotel will be no exception. It's a fact that tourist to Bathurst predominantly use car transport.

To accept the argument that 99 rooms of hotel can be built without any car parking is ludicrous. How would Council assess any standalone hotel/motel development re parking needs? Similar logic must be applied to this application. Indeed,

Such hotel parking needs in and around Keppel St would seriously disadvantage local residents and visitors. Also the potential for hotel patrons to “overflow” to the rail station car park is significant, potentially impacting rail commuter parking and overall precinct amenity.

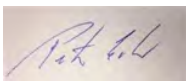
WASTE AND SERVICE VEHICLE ACCESS AND PARKING

The application seeks to have all waste removal done from the kerbside, with no on site access. Noise, kerb blockage and other impacts to local residents and adjoining premises must be a concern. For a development of this capacity with hotel, retail, business and function/event capability this is unrealistic. Service vehicle access and parking must also be included on site.

VIABILITY OF ADDITIONAL RETAIL AND FOOD & BEVERAGE SPACE

We would like to note our concerns with the proposal for additional retail and food and beverage capacity within the site. The Keppel St retail strip is already struggling with numerous vacant premises. Also, a number of retail business operating from the Victoria Stores building on the subject site have all failed to be viable over the last few years (including pre-COVID). For the consideration of Council's business and economic planners also.

Sincerely



Peter Eeles

G. Eeles

Glenda Eeles

29 Coolabah Close
Kelso NSW 2234

Emma Castle

From: Bathurst Regional Council <council@bathurst.nsw.gov.au>
Sent: Sunday, 29 January 2023 8:15 PM
To: Council
Subject: Contact Bathurst Regional Council

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Name

angus mckibbin

Email

angus.mckibbin@bigpond.com

Contact Phone Number

0408825988

Address if Applicable

131 Stewart St Bathurst 2795

Subject

Tremain's Mill development Application

Message

I strongly object to this development. It is total overkill. The Hotel is out of proportion to all the surrounding historic buildings. There are totally insufficient car parking spaces. This area was once to 19th Century gateway to Bathurst following the arrival of the train in 1876. Nearly all the buildings up Keppel street from the Railway Station reflect this period, This development will destroy this unique microcosm of 19th century Bathurst.

File Upload

Wiradjuri Country
W: www.bathurst.nsw.gov.au



Bathurst Regional Council acknowledges the Wiradjuri People as the Traditional Custodians of the land in the Bathurst Region. We pay respect to Elders, past, present, and emerging and acknowledge all Aboriginal and Torres Strait Islander people who live, work and play in the Bathurst region.

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Emma Castle

From: Bathurst Regional Council <council@bathurst.nsw.gov.au>
Sent: Monday, 30 January 2023 11:43 AM
To: Council
Subject: Contact Bathurst Regional Council

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Name

Wendy Hastings

Email

wendyhastings@tpg.com.au

Contact Phone Number

0428319501

Address if Applicable

20 Casey Circuit

Subject

Keppel Street east development

Message

Hi I have tried to make a submission re the Your Say portal but there is no link for this project. I am opposed to the building of a multi-storey hotel on the site near Tremain's mill. Currently that green space is utilised by a range of members of the community - to loose it in total would be an issue. I think developing the eastern side and the towers is a good idea but the hotel is not defensible. It will seriously overshadow the Early Childcare centre and surrounding residences. Parking is a problem; Bathurst has many empty shops already and more are not needed. Traffic will become a problem in what is a lovely artsy precinct, totally changing the nature of the area. Development is good but not at the expense of the overall ambience of that area.

File Upload

Wiradjuri Country
W: www.bathurst.nsw.gov.au



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Attachment 8.2.3.9

BATHURST REGIONAL COUNCIL
30 JAN 2023
REF. 2022/523/081

TO: Bathurst Regional Council
158 Russell St
Bathurst NSW 2795

RE: Proposed development of Tremain Mill (Development Application No 2022/523)

Dear Council

I am a resident of a street near this proposed development.

I object strongly to the proposal on the grounds that it does not provide enough parking.

The development application states that a development with 90 hotel rooms and 28 shops, like this one, should have more than 200 on-site parking spaces.

But the application does not include any new parking spaces and states that the surrounding streets will provide enough parking within 200 to 400 metres of the site.

This includes the street I live on. I do not want to park 400 metres away from my house because my street is filled with the cars of hotel guests.

I urge council to require the developer to put all the required parking on the Tremain Mill site.

Yours sincerely

Rosalie Davis 21 Keppel Street
21 Keppel Street
Bathurst C

Robert Martin
21 KEPPEL ST.

DEPBS

Initial Comments on Development Application DA/CC 2022/523

Tremains Mill Complex – Keppel, Manilla and Havannah Sts

1. The building heights create too much shadow over adjoining land and buildings even those right across a wide street which is unreasonable, it also blocks sun from the streets and their own site for our climate. This shadow will prevent survival of the proposed landscaping plants and gardens as well as creating cold and drafty laneways within the development. This will not be conducive to lingering, general walking or any form of outdoor activities within the open spaces. It will also make residence or business in those buildings more uncomfortable and less healthy for those living, being in care at the Childcare centre or working in adjoining premises.

The new building heights that exceed the current LEP should be redesigned to fit within the LEP which was created to provide proper and appropriate levels of development for the area. What point having these planning instruments if developers can be allowed to routinely ignore them as is occurring in the current approvals periods and processes we are experiencing. Council may as well throw all the planning codes and instruments out the window as allowing any further additional approvals exceeding the limit will mean that there is no longer any control over the limit at all and we loose complete control over the towns future development. It makes a mockery of having any codes of development.

2. The density is too great for the area of the site and will create significant traffic and parking issues as the site plans to not provide for anywhere near enough parking to accommodate this huge increase in use and density. In fact there is no area within Keppel Street that anywhere near that level of density and the street is primarily heritage and low in visual impact on the streetscape even the existing buildings on the street front of the Tremains Mill and Victoria Hotel.
3. There is nowhere near enough onsite parking and this must be addressed as most of the community are vehicle reliant as would the majority of travelling visitors who would stay at the hotel. There is no identification as to how this issue can be address within the local area of this proposal and that must be provided and costed as part of this development as well as funded before this can be approved as currently presented.
4. There is no provision for the Milling Museum which has been a prime plan for the site for quite some time. Where and how does the developer propose properly including this history in the site which would make it a much more community and visitor engaging feature.

It's quite clear that the prime and only focus of this development is commercial return, the percentage of open community space within the site is very minimal, limited mainly to laneways between buildings, where has the community focus it currently contain been properly and adequately addressed.

...2/

2/...

5. The green line in diagram 2 on page 14 of the Concept Master Plan is a joke. It has absolutely no relevance to how this development will impact on the immediate site on which it is proposed and the actual block in which it sits. The line may as well show the height of this proposal in relation to Mt. Victoria.

There is so much more that should be said and needs to be said in regards to this proposal, this is the mere tip of the issues that need greater examination and negotiation.

Unfortunately the Council's system would not enable download of the documentation until finally on Sunday the system became properly accessible. This is something that Council need to ensure is not a recurring issue with information for proposals. Time Council upgraded systems to cope with this issue as a propriety as it is an issue in providing proper fairness and access for the community to the information within a timely manner to enable the community to prepare thorough submissions.

Submitted by Jennifer Gray

30 January 2023

The General Manager
Bathurst Regional Council
158 Russel Street, PMB 17
Bathurst NSW 2795
By Email: council@bathurst.nsw.gov.au

Dear Sir

Re: DA No 2022/523 – Proposed Development, Concept DA – Redevelopment of Tremain Mill

Thank you for the opportunity to respond to the above proposed Concept Development Application (DA) to redevelop the Tremain’s Mill site.

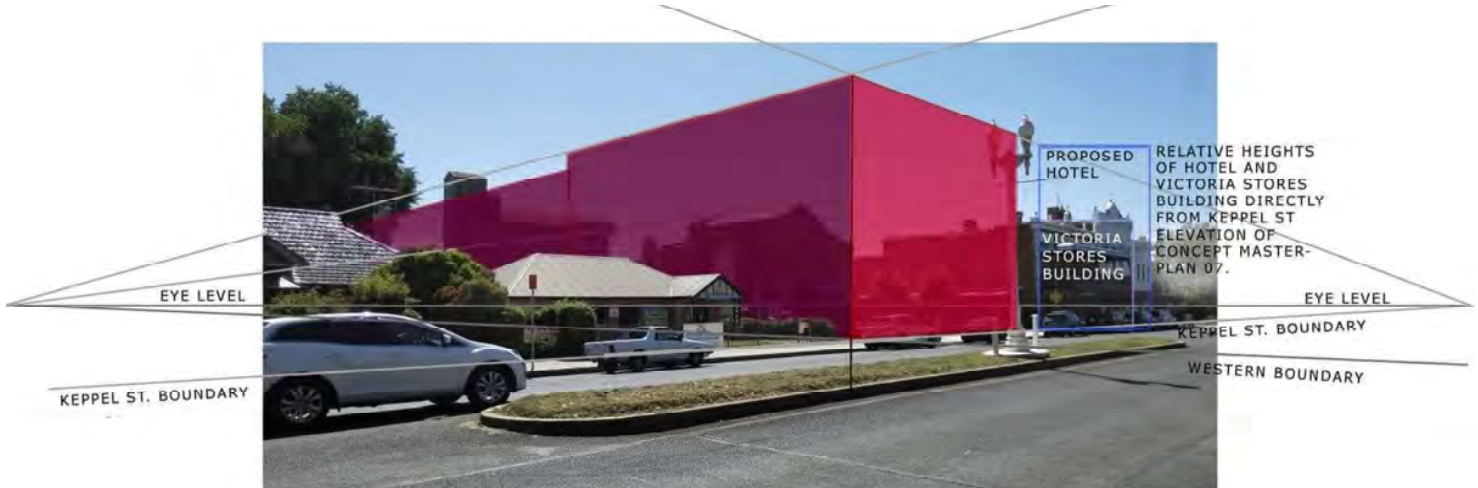
Our relationship to the Tremain’s Mill

We own and have lived in our terrace home diagonally opposite the proposed development for over 25 years, and rented our home for another 10 years prior to that.

We, our friends, visitors and clients meet and socialise in this lovely area of town. We love the existing visual/spatial contributions of the Tremain’s Mill site and buildings to our own context of home and daily living. It goes without saying the Tremain’s Mill site and its buildings are a vital part of the wider neighbourhood’s – and the town’s - charming historic aesthetic and sense of space and place that is so attractive to residents, visitors and users of lower Keppel/ Havannah Streets and surrounding neighbourhoods.



Importance of Tremain’s Mill buildings to the streetscape/skyline from our front door



Attachment 8.2.3.9

The previous owners' created a precinct that had become a well-used and much loved place of art, culture and music. Keppel (and Havannah) Street has been given light shows on the iconic stairwell wall of the brick mill and the silos. Community-driven art shows held in the Bedwells barn's iron shed. Musicians play in its open space. The community and their children (and pets) safely enjoy a village meeting space in winter sun in the open grassed "park" that fronts Keppel Street, and which so successfully retains the open street vista to the site's beautiful industrial heritage buildings and its relationship to the adjoining open space in the heritage Railway precinct.

It goes without saying the Tremain Mill site and its buildings/silos have been **and continue to be key Landmarks** to the town's post-colonial history, social stories and built heritage for most of the town's 208 year existence.

Their prominence and visibility plays a significant part of our heritage horizon-scape, and they are iconic place-markers for the surrounding Keppel Street, Railway and Milltown precincts in which the site sits.

Their visual/spatial relationship to neighbouring and neighbourhood listed (and unlisted) heritage buildings and places is a vital contribution to our streetscapes. The importance of their visual aesthetic continues to be recorded by artists and photographers who we regularly note in Keppel, Manilla or Havannah Streets capturing the site's lines, colours, shapes and spaces.

The significance of these buildings is acknowledged in the Concept DA SoEE and HIS, with the HIS further quoting the 2016 Conservation Management Plan for the site, which states:

"... The largely intact surroundings free from large scale modern development contributes significantly to the historic character and setting of the former Tremain's Flour Mills. The mill complex in turn contributes significantly to the Bathurst conservation area."

"The former Tremain's Flour Mill is therefore of significant local heritage with historic, associative, aesthetic, social, rare and representative values. The property is relatively intact and has a high degree of integrity. Owing to its integrity and intactness, and the strength of its values, the property may be potentially suited for listing on the NSW State heritage register."

We note the avenue for protection on the NSW State heritage register has not been pursued, and that if the Concept DA is approved the damage to the "historic, associative, aesthetic, social, rare and representative values" of the site and its iconic contribution to our heritage streetscapes will be substantial.

The assertions of the proponent's paid consultants that this proposal to jam in a six storey hotel, three apartment blocks, and two commercial blocks including 28 shopfronts – all of which contravene the height restrictions and car parking (with regard to the hotel and commercial spaces) requirements in the LEP - will somehow enhance or feature the historic values of the site, and benefit the neighbourhood, is absurd.

Objects of the *Environmental Planning and Assessment Act 1979* (s1.3)(EP&A) include, among other things:

- (f) To promote the sustainable management of built and cultural heritage (including Aboriginal cultural heritage),
- (g) to promote good design and amenity of the built environment, and
- (j) to provide increased opportunity for community participation in environmental planning and assessment.

The Concept DA as it stands fails in this regard from the outset.

The NSW Government Architect's *Better Placed*¹ design policy "provides clarity on what the NSW Government means by good design" as required by the EP&A. It includes "not just how a place looks, but how it works and feels for people, and the processes for achieving this". Its accompanying guide *Urban Design for Regional NSW*² reiterates the Objectives which we reference below:

- The height, mass and density of the new buildings do not engage, enhance, respect or sit sensitively within the existing built heritage and its contextual spaces, nor with the storied history of the site and its iconic heritage buildings.
- The proposed mass, height and footprint of the development/new buildings do not relate to the area, neighbourhood or surrounds, or respond to the context in which they are designed. The bulk and height – especially of the Keppel Street fronted hotel - are not an appropriate and complementary addition to the heritage precinct, neighbouring heritage-listed buildings, and urban/residential streetscapes.
- The project is not responsive to the needs, use and amenity of surrounding homes/neighbourhoods, existing businesses and services, and wider community enjoyment and value of this important heritage site and wider heritage precinct. Further, without community consultation and input, it can't claim otherwise.

The proposal as it stands also contravenes Schedule 4 of the *Bathurst Regional Development Control Plan 2014* that determines that within the conservation areas of Bathurst, infill development *must complement and enhance* the local character by relating to the predominant *scale, massing, setbacks, colours and materials of the area* – and the adjoining developments to each side.

It further contravenes the *Bathurst Region Local Strategic Planning Statement (LSPS) Vision Bathurst 2040* objective to "protect and enhance the region's ... heritage, including its built heritage".

The height, bulk and footprint of the proposed new buildings also undermines the findings of the *Bathurst Region Destination Management Plan 2015* which finds that:

- Bathurst's heritage architecture and historical places and landscapes are *Key Assets*
- History is most acknowledged by visitors "due to the streetscape and the prominence of older buildings in the city centre" and that "there is strong community value regarding the heritage of the region" and "**preservation of the historical streetscape is seen as essential to the future of the region**"
- "The market views the region as beautiful. **As the city continues to grow, it will be important to protect the aesthetic values of the area, including the rural aspect and the heritage streetscape.**"
- Bathurst's history and heritage is identified as vitally important to the Bathurst region brand, and that one of Bathurst's "primary target markets is Cultural visitors"
- Place planning and promotion priorities identify Keppel Street as an arts precinct, with its co-located [historical] Railway precinct.

Protection of our built heritage and streetscapes is in fact a recurring top priority in most relevant planning documents, strategies and reports that guide the development and direction of the city's future.

The Concept DA contravenes a number of important clauses of the Bathurst LEP that are there to protect the public interest and to guide sensitive development of places. Approval of the Concept DA would require that

¹ NSW Government Architect, *Better Placed – An Integrated design policy for the built environment of NSW*, NSW Government Architect (2017)

² NSW Government Architect, *Urban Design for Regional NSW – A guide for creating healthy built environments in regional NSW* (2020), NSW Government Architect (2020): This document supports the above *Better Placed* policy and provides key considerations to "guide and evaluate the design of the built environment through local planning, design, and development". It states "good design outcomes depend on good design process" which includes collaboration, consultation and responsiveness to place - involving community members and organisations, local businesses etc. This has been absent in this proposal

the proposed contraventions of the Bathurst LEP are in the public interest.³ The Concept DA grossly fails this on a number of significant counts. The tone-deafness of its spatial footprint to the context and values of the site, its neighbourhood, and the community use of the area, is profound. It signals an equally blind approach to modern design that could be used to magnify and complement the significant heritage and visual values of the site and its buildings. This is hinted in the description of “plain” building blocks which are asserted will promote and celebrate the heritage buildings in the streetscape despite these features being obscured, demolished or overpowered by the proposed new building footprints.

We provide the following that includes our lived experience of our neighbourhood and community in this part of the city, and the effect on private and public amenity and use of space and place. We note that diminishing of such amenity has also a financial consequence for the businesses and services that currently enjoy the existing amenity and flow-on effects into the town.

Heights & bulk

The site sits at the very edge of the B3 Commercial use area in a conservation area, both of which have a height limit of 12 metres. They also sit within a neighbourhood of R1 Residential use area and homes, where the height limit is 9 metres. The highest buildings adjacent to and in the neighbouring streets are two storeys. All but one of those two-story buildings are built heritage that have visual relationship to the heritage buildings on the site. In fact, the two homes opposite the site were built by the Tremain family as their family homes, where they could oversee the site and its workings.

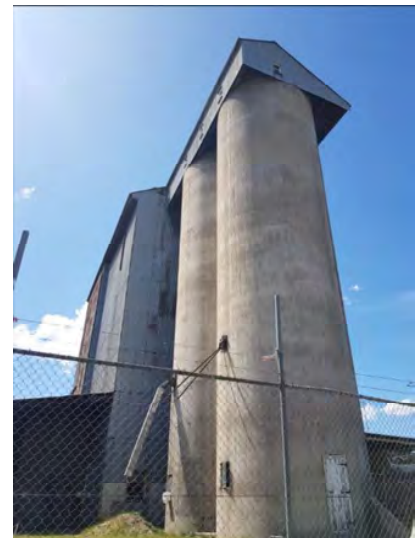
The height of the silos especially and the brick Mill building are part of what makes them iconic heritage landmarks and place-markers for the town. They are original to the site and the area, and are contextualised by the visual breathing space around them and in the surrounding streetscapes. This ensures their heights also do not overwhelm the surrounding heritage-rich precincts or adjacent heritage buildings and homes and lived neighbourhood in which the site sits.

Their setback ensures homes, buildings and streets/footpaths around them are not plunged into winter shadow for much/most of the day, nor their own daily amenity/ enjoyment of use overwhelmed by their bulk.

The non-compliant height and bulk of the new buildings vastly fail in this regard. Jamming the silos and mill buildings between equally high new blocks of buildings does not feature or enhance the silos and the other heritage listed buildings, **but conceals them and destroys their context**. The town loses sight of these iconic heritage landmarks and the streetscapes/neighbourhood from all sides will be overshadowed and overpowered by these new buildings.

To assert otherwise is nonsense.

We add that part of the context of the place is its surrounding space. This is an integral part of the heritage of the site. Filling every available opening full of buildings is in complete contravention of the historical context of the space. The current use of the forecourt where families and friends enjoy coffee or a meal in the sun, with kids playing on the lawns surrounded by heritage buildings, respects and enhances the existing context.



The hotel fronting Keppel St is proposed to be the height of the silo gutterline

³ Bathurst Regional Local Environmental Plan 2014, clauses 4.6(4) and 4.6(5)(b)-(c) viewed at: <https://legislation.nsw.gov.au/view/html/inforce/current/epi-2014-0729#sec.4.6>

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A building fronting Keppel Street the height of the silo's gutterline and the width of the existing (and original to the site) open space - with just a shadowed four metre alley between it and the heritage Victoria Stores building - is wildly inappropriate. It will loom over Keppel Street and its neighbouring kindergarten. Its height and placement will dominate the streetscape and its skyline, and diminish the heritage buildings not just adjacent to it but in the whole block. It will remove the sight of the silos, the Mill building and the Bedwells building from the street and from the neighbourhood and its residents, visitors and users. Contrary to the Concept DA's assertions, the proposed 4m wide alley from Keppel Street will not provide views of the heritage features of the site.

The proposed terraces and apartment blocks' non-compliant heights will visually overpower Manilla street and the homes opposite and contribute to the overall unacceptably huge bulk of the whole proposal.

The non-compliant height of the apartment block (reaching the gutterline of the silos) accessed from the Havannah Street side of the silos will likewise do the same, and remove those landmarks and their iconic shapes from the streetscape rendering them into a barely visible afterthought. Their marking of the Havannah Street approach and arrival to the heritage, cultural and tourism Railway, Milltown and Keppel Street precincts will be diminished and obscured by an equally tall new apartment block.

The non-compliant heights of the two new commercial building blocks will likewise obscure even more of the significant Tremain heritage buildings from the streetscape, and their height, bulk and placement will overpower the streetscape and diminish the presence of the adjacent two-storey Victoria Hotel and Railway Station.

The Concept DA's bulk, height and placement of all new buildings create an imposing physical and visual barrier to the site and vastly diminish the enjoyable experience of strolling the neighbourhood – which is a widely-enjoyed recreation in this part of town due to its open and inviting streetscapes.

There are also major privacy issues. A six-storey hotel on Keppel Street will overlook straight into our upstairs bedroom windows/French doors and verandah that we use on a regular basis as a place to work and live - especially in the winter morning sun that is so rare to access in our terrace homes. Other adjacent terraces variously use these upstairs rooms as general living spaces and bedrooms. Blocking our windows or closing up our verandahs to avoid hotel clients looking into our bedrooms/living spaces, will leave us effectively without window space to an outside view.

We know from conversations that the pre-school adjacent to the proposed hotel, *and its parents*, are most concerned at the overlooking of their children's preschool and play area (coupled with the imposing bulk of the building that will loom over their childrens' daily space).

Overshadowing

The shadow plans confirm that neighbouring land, homes and businesses will be overshadowed, with winter shadows reaching across the whole street across the homes and their gardens, and into the back lane of lower Keppel Street and then across the Victoria Hotel's much used open beer garden and over Havannah Street homes.

The shadow maps do not include the deep pre-9am shadows that will reach even deeper across the homes, street and footpaths on the north-western side of the site, nor the post-3pm shadows that do the same across Havannah Street – where the popular beer garden of the Victoria Hotel, with its established and well-patronised live music scene, will be plunged into shadow most of the day.

These Keppel and Havannah Street footpaths are well-traversed and regular strolling routes for the community and for tourists driving through. They are open space that entice people to stay longer and

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venture into the town proper, and are meeting and conversation places for the residents and community enjoying perambulating the area. This is particularly so during winter, when the footpaths are largely bathed in sun for much of the winter day.

The proposed heights, bulk and placement will also cast even deeper winter shadows over the adjacent childcare building and play area and homes to its northwest.

The internal open spaces and alleys of the proposed development will also be overshadowed most of the day, coupled with looming heights surrounding them. Given the prevailing wind direction, the 4m wide alley between the proposed 80 room hotel and the Victoria Stores building will wind-sweep winter leaves into those internal spaces.

This is a far cry from the promotional sketches provided to market the proposal, which shows people enjoying sunny café tables in inviting open courtyard settings, and a far cry from the present sunny and inviting green spaces fronting Keppel Street.

It is also anathema to daily wellbeing and invitation to use spaces and neighbourhoods. Familiarity with the sunless and imposing Sydney CBD spaces confirms the lack of use or lingering in those spaces by community members (or indeed shoppers).

Creating massive shadows is also incompatible to the growing need to maximise sunlight for passive heating and generation of solar-power that is increasingly needed with energy costs and an urgent global need to maximise the use of renewable power.

Conversely, the huge increase of light during the evening from those densely roomed new buildings will further demand attention away from the heritage buildings and streetscapes – and the primacy of the heritage-listed streetlamps that define Keppel Street. They will remove what night-dark exists from the neighbourhood – especially given bedrooms are largely located at the front of the homes facing the site.

Parking & traffic

The arguments supporting to not provide the 200+ mandated parking needs of a proposed 99 new hotel rooms, 28 shops, two commercial spaces, and function and event spaces are frankly offensive to us residents and regular users of the area.

Even the provided 46 residential parking spaces will be insufficient for those new residences given most couples and households have at least two cars. This is already the case on Keppel Street for residents.

The assertion that there is “ample” on-street car parking spaces by the Traffic Impact Assessment confirms either a lack of familiarity or a disdain for the lived experience of the neighbourhood and existing use of the precinct, and a complete lack of consultation.

The current parking availability in lower Keppel Street is very regularly unavailable. We experience this sometimes several times a day. It is not unusual for us to have to find a park hundreds of metres away from our own home. This is particularly so on those days/evenings when the Victoria Hotel’s popular live music offerings attract fans and musicians from as far as Sydney. Just yesterday, on Sunday evening, we noted that the live music event held in the Victoria Hotel’s beer-garden saw no spare on street car parking for the first two blocks of Keppel Street, and a block either side along Havannah Street.

When community events are held at the showground, Machattie Park or the football fields – there is no parking in the surrounding streets of Keppel, Havannah, Manilla, Seymour and Russell Streets, including the carparks behind the library and the Neighbourhood Centre. This has occurred also during the car races on the Mount when visitors to town have parked their cars in the area. We have had to abandon returning home with fortnightly shopping on one occasion when an event in Machattie Park found the nearest car

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space over 3 blocks away in *every direction*. This is not acceptable for residents, let alone for the elderly, injured or unwell, with shopping, or in extreme weather. It is also contrary to our car insurance covers that confirm our cars are parked at our addresses.

Seven years of weekly train commuting to Sydney for work also confirms that those car parks reserved for train customers are oversubscribed during departure/arrivals. This also creates risk for passengers' ingress and egress to/from cars and buses. It is inevitable that hotel clients, visitors, residents and shoppers to the proposed development will put further pressure on those carparks as well as existing and insufficient street-parking.

We express also our strong concern for the parents and children of the Keppel Kindy, whose safety from busy traffic and over-subscribed street parking – especially in the mornings when the streets are full – must be protected.

We further wish to comment on the Traffic Impact Assessment's assertion that "parking credits" of 165 car parking spaces exist on the current floorspace and use of the existing buildings.

The proponent would well know that the Bedwells Barn is not an entertainment facility or function centre. Further the small tin section to its right side has been used for small community-organised art shows just a handful of times over the past seven years. The buildings asserted to be used for business or offices have been mostly unused over that time. The Mill building has only ever been used as storage or quiet private studio space, and has been largely unused and empty for years. The Manilla Street shed has been unused for years, and rarely used before that. The two shops that front Havannah Street have been untenanted for well over a year, if not more.

The rest of the site has never been used to the extent it has required extra parking, other than the industrial historical use of the site for milling and storage of wheat, or the pre-2016 use of the site for Bedwells Feed Barn, where likewise most of the site was used for storage and the heritage machinery used to sort seeds and grain. There has never been a "parking credit" of any size, and to assert otherwise is absurd.

Further, the assertion people will walk to the shops shows a fundamental lack of understanding of how rural communities function. People do not walk, bus or bike into town for shopping, work, recreation or for events from Kelso or Eglinton, let alone from local communities like Rockley or Sofala, or regional population centres like Lithgow or Orange. People will drive to this new shopping centre, and they will need somewhere to park their cars.

Visitors and guests of the hotel room will likewise not be catching the rare train or bus to Bathurst, let alone with their luggage or as part of their exploration of the town and its region. The further suggestion that an electric bus from Circular Quay somehow constitutes the provision of car parks to the area is nonsense.

We also note the recent DA for a gym onsite was refused because it was a few car spaces short. The existence of "parking credits" onsite is nonsense.

The Traffic Impact Assessment is inadequate and based on incorrect information.

Further, the enticement of substantial financial contribution to Council in lieu of provision of mandated car space must be resisted. The EIS acknowledges that any carparks built elsewhere in the city would not alleviate the parking needs of the development and "any potential contributions would be unlikely to benefit the site directly." We refer to the issues raised about the effects on residents, businesses and services in the neighbourhood as described above. Permitting a lack of required car spaces would be a failure of Council planning that only grows as the city grows into the future.